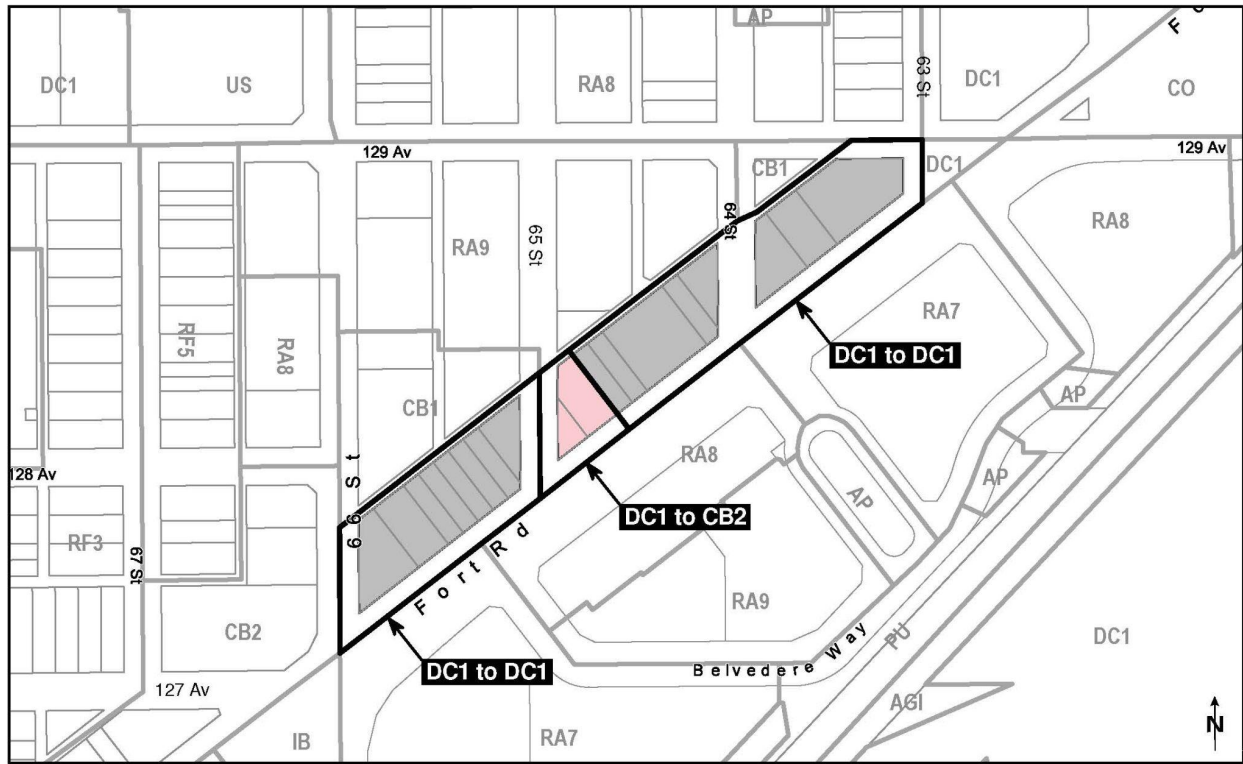




ADMINISTRATION REPORT REZONING, PLAN AMENDMENT BELVEDERE

12720, 12732, 12740, 12742, 12744, 12748, 12754, 12758, 12804, 12812, 12816, 12816A, 12834, 12844, 12866 and 12890 - Fort Road NW, and 12820 - 64 Street NW

To allow for commercial development and update DC1 Provision to remove the two rezoned lots.



Recommendation: That Charter Bylaw 19936 to amend the Zoning Bylaw from (DC1) Direct Development Control Provision to (DC1) Direct Development Control Provision and (CB2) General Business Zone be **APPROVED**.

Administration is in **SUPPORT** of this application because:

- it supports City Plan policies for Secondary Corridors and Non-residential lands; and
- it facilitates redevelopment of Fort Road in accordance with local plans.

The Application

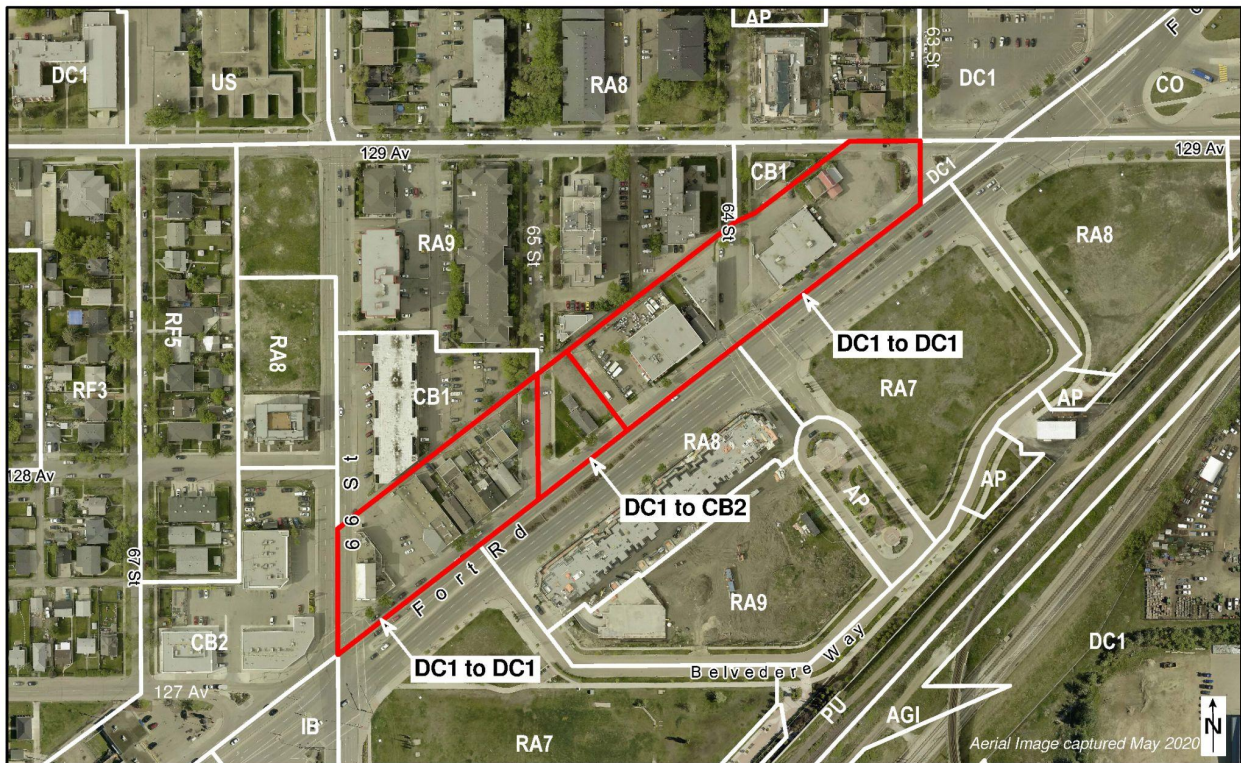
CHARTER BYLAW 19936 proposes to amend the Zoning Bylaw to (CB2) General Business Zone and provide administrative updates, including a boundary adjustment to the (DC1) Direct Development Control Provision for the properties that remain.

The applicant represents the owner of the two properties proposed for rezoning to CB2 in order to develop a Child Care Service Use as part of redevelopment of the site.

Site and Surrounding Area

The existing DC1 Provision consists of 17 properties along three block faces fronting Fort Road, between 66 and 63 Streets NW. Some of these properties have older developments, some of which are buildings identified in the City of Edmonton's Inventory of Historic Resources; and some are vacant lots. The DC1 area has not been redeveloped as envisioned in the DC1 Provision (approved September 10, 2007) as per the aspirations of the Fort Road Implementation Plan that resulted from the approval of the Fort Road Old Town Master Plan in November 2002.

The Belvedere Transit and LRT Station are immediately north of 129 Avenue, east of Fort Road, approximately 550 m from the proposed rezoning.



AERIAL VIEW OF APPLICATION AREA

The following table refers to the two properties proposed for rezoning to CB2:

	EXISTING ZONING	CURRENT USE
SUBJECT SITE (Proposed CB2)	<ul style="list-style-type: none"> (DC1) Direct Development Control Provision (12804 and 12812 Fort Road) 	<ul style="list-style-type: none"> Undeveloped
CONTEXT		
North, across lane	<ul style="list-style-type: none"> (RA9) High Rise Apartment Zone 	<ul style="list-style-type: none"> 3.5-storey Apartment Building
East	<ul style="list-style-type: none"> (DC1) Direct Development Control Provision (part of existing DC1, Area F) 	<ul style="list-style-type: none"> Undeveloped
South, across Fort Road NW	<ul style="list-style-type: none"> (RA8) Medium Rise Apartment Zone 	<ul style="list-style-type: none"> 5-storey Apartment Building
West, across 65 Street NW	<ul style="list-style-type: none"> (DC1) Direct Development Control Provision (part of existing DC1, Area F) 	<ul style="list-style-type: none"> Retail commercial



VIEW OF PROPOSED CB2 PROPERTIES LOOKING NORTH

The following table refers to the balance of the existing DC1 properties:

	EXISTING ZONING	CURRENT USE
SUBJECT SITE (Administrative Amendments)	<ul style="list-style-type: none"> (DC1) Direct Development Control Provision (fifteen properties) 	<ul style="list-style-type: none"> Mix of older Commercial Buildings and undeveloped sites
CONTEXT		
North, across lane	<ul style="list-style-type: none"> (CB1) Low Intensity Business Zone (RA9) High Rise Apartment Zone (CB1) Low Intensity Business Zone 	<ul style="list-style-type: none"> Commercial Building 3.5-storey Apartment Building 10-storey Apartment Building mixed use Building
East, across 63 Street NW	<ul style="list-style-type: none"> (DC1) Direct Development Control Provision (Area G) 	<ul style="list-style-type: none"> Road right-of-way
South, across Fort Road NW	<ul style="list-style-type: none"> (RA7) Low Rise Apartment Zone (RA8) Medium Rise Apartment Zone (RA7) Low Rise Apartment Zone 	<ul style="list-style-type: none"> Undeveloped 5-storey Apartment Building Undeveloped
West, across 66 Street NW	<ul style="list-style-type: none"> (CB2) General Business Zone 	<ul style="list-style-type: none"> Commercial Building

Planning Analysis

PLANS IN EFFECT

The rezoning area is within the Belvedere Area Redevelopment Plan (ARP) and the Fort Road Old Town Master Plan. These plans support commercial development with opportunities for residential development. The Belvedere ARP identifies Fort Road as a Pedestrian Priority Zone and supports commercial revitalization and the development of an urban village.

THE CITY PLAN

This proposal aligns with the applicable policies of The City Plan that focus on secondary corridors and non-residential areas. The subject properties are located within the Northeast District of the City Plan and are identified as Established Non-residential. Fort Road is considered a Secondary Corridor and is intended to be a vibrant street, smaller in scale to Primary Corridors with a more residential character, some commercial clusters, and local destinations for surrounding communities.

LAND USE COMPATIBILITY

The proposed CB2 zoning is compatible with the existing DC1 Provision as well as the surrounding mix of residential and commercial zones and uses.

The Main Street Overlay, Section 819 of the Zoning Bylaw, will apply to the proposed CB2 properties. The purpose of this Overlay is to encourage and strengthen the pedestrian character of Edmonton’s main street commercial areas that are located in proximity to residential and transit-oriented areas by providing visual interest, transparent storefront displays, and amenities for pedestrians.

The proposed rezoning to CB2 will allow for additional commercial development opportunities on the subject site with appropriate development regulations and support reinvestment in the Fort Road area.

DC1 & CB2 COMPARISON SUMMARY

	DC1 Current	CB2 Proposed
Principal Building	Main Floor Commercial with Residential required above 2nd floor	Commercial (Residential not required, Discretionary)
Overlay	n/a	819 Main Streets Overlay
Height	18 m	14.5 m flat roof/

		16.0 m pitched roof 18 m for Apartment Hotel or Hotel Use
Maximum Floor Area Ratio (FAR)	2.0	3.5
Front Setback (fronting Fort Road)	No Setback is required	No Setback is required where commercial buildings abut the property line of a pedestrian oriented street* 1.0 m to 2.5 m**
Interior Side Setback (abutting DC1)	No Setback is required	No Setback is required
Flanking Side Setback (abutting 65 Street)	No Setback is required	4.5 m Abutting a public roadway 1.0 m to 2.5 m**
Rear Setback (residential across an abutting lane)	2 m	6 m where the Rear Lot Line Abuts a residential Zone Site Abuts a lane, Not Required
Landscaping	10% more than Section 55	As per Section 55
<p>Notes:</p> <p>*Fort Road is intended as a Pedestrian Main Street as per the Old Fort Road Master Plan and is identified as a Pedestrian Priority Zone in the Belvedere ARP</p> <p>**CB2 Regulation is amended by Main Streets Overlay (Section 819)</p>		

In summary, the main differences between the two zones include: a reduced Height under the proposed CB2, residential is not a required component under CB2 as it is with the DC1, the CB2 allows for more commercial use opportunities, and the CB2 Zone does not require incorporation of the historic facade as does the DC1.

Technical Review

TRANSPORTATION

Transportation advised the following:

- Vehicular access to the CB2 site must be from the lane only and that vehicle access to Fort Road will not be permitted.
- Lane improvements will be required upon redevelopment, including paving to a commercial alley standard and/or removing/relocating utility poles that may interfere with access.
- The south corner of the proposed CB2 site includes a currently paved portion of the pedestrian realm. Dedication of this area as road right-of-way to maintain public access is desired and will be reviewed at the development permit stage.

DRAINAGE

Drainage advised the following with regards to the proposed CB2 Sites:

- Sanitary servicing is available from the combined sewer main within Fort Road.
- No storm sewer service connections exist and redevelopment of the site for commercial purposes will require connection.
- Permanent storm servicing is required and is available from storm sewer mains within 65 Street NW or Fort Road NW.
- Onsite stormwater management requirements include storage provisions within the property to accommodate the excess runoff in a 1 in 100 year design rainfall event.

Drainage advised the following with regards to the proposed DC1 Sites:

- With redevelopment, existing sanitary servicing would need to be abandoned and provided from the combined sewer mains within Fort Road NW.
- Stormwater servicing would need to be provided from the existing separate storm sewer mains in the area.
- Onsite stormwater management requirements include storage provisions within the property to accommodate the excess runoff in a 1 in 100 year design rainfall event.
- Servicing and stormwater management requirements will be further assessed at the development permit stage.

EPCOR WATER

EPCOR Water advised the following:

- that existing services may not be of sufficient size for new development and that a review of the total on-site water demands and service line capacity should be conducted with a qualified engineer;
- existing lead services (to lots 32 and 34) cannot be re-used and new services connections must be constructed with any new development;
- there is a significant deficiency in on-street hydrant spacing (distance between fire hydrants) adjacent to the properties (City of Edmonton Standards requires hydrant spacing of 90m). A warning regarding the deficiency is required on each titled parcel. An Infill Fire Protection Assessment (IFPA) will be required at the time of development to determine if additional fire hydrants are required;
- to prevent cross lot servicing, consolidation of titles may be required.

All other comments from affected City Departments and utility agencies have been addressed.

Community Engagement

<p>ADVANCE NOTICE August 13, 2021</p>	<ul style="list-style-type: none"> ● Number of recipients: 271 ● Two responses were received
<p>WEBPAGE</p>	<ul style="list-style-type: none"> ● https://www.edmonton.ca/residential_neighbourhoods/neighbourhoods/belvedere-planning-applications

Of the two responses received, one was to request clarification on the application and the other was to ask if Child Care Services means Daycares.

Conclusion

Administration recommends that City Council **APPROVE** this application.

APPENDICES

- 1 DC1 Track Changes
- 2 Application Summary

(DC1) DIRECT DEVELOPMENT CONTROL PROVISION (AREA F)

1. Area of Application

~~This Provision shall apply to Lot 2-5, 6-9, Block 3, Plan 3600U; Lots 14, 15, 16-19, 20, 21, Block 7, Plan 2552S; and, Lot 30, 31, 32, 33, 34, 35, 36, 37, 38, and 39, Block 2, Plan 2552S. The area is located along the northwest side of Fort Road generally between 66 Street and 63 Street,~~ designated as Area F and shown on Appendix I.

2. ~~General Purpose~~ Rationale

To accommodate an infill and intensification of the existing Fort Road commercial area as part of a transit-oriented, residential mixed use development that creates a liveable "urban village" environment through the retention of identified heritage structures, use of urban design regulations, and the introduction of a diversity of housing types within walking distance to the Belvedere LRT Station. Commercial uses will be developed on the ground floor of a number of the buildings as part of this development project.

3. Uses

- ~~1. Apartment Housing~~
- ~~2. 1 Minor Home Based Business~~
- ~~3. 2 Residential Sales Centre~~
- ~~4. 3 Bars and Neighbourhood Pubs, for less than 100 occupants and 120 m² of Public Space.~~
- ~~5. 4 Business Support Services~~
- ~~6. 5 Convenience Retail Stores~~
- ~~7. 6 General Retail Stores, not to exceed 3000 m²~~
- ~~8. 7 Health Services~~
- ~~9. 8 Hotels, limited to the existing Transit Hotel~~
- ~~10. 9 Major and Minor Amusement Establishment as an accessory use to a restaurant~~
- ~~10. Minor Alcohol Sales-Liquor Stores~~
11. Multi-unit Housing
12. Personal Service Shops
13. Professional, Financial, and Office Support Services, excluding loan offices and similar financial uses
14. Restaurants, for less than 200 occupants and 240 m² of Public Space
15. Specialty Food Services, for less than 100 occupants and 120m² of Public Space
16. Fascia On-premises Signs
17. Temporary Freestanding On premises Signs, limited to project identification, building construction identification and real estate advertising signs during construction and initial sale excluding portable signs.
18. Projecting On-premises Signs

4. Development Regulations

1. The Developer shall provide proof satisfactory to the Development Officer, in consultation with Alberta Environment, and the Transportation and Streets Department

that the lands have been remediated to allow the intended use prior to the issuance of a major development permit.

2. Development applications for new buildings and major additions are required to go before the Edmonton Design Committee prior to the issuance of a development permit.
3. The overall Site development shall be in accordance with the urban design criteria established herein and in the Fort Road Old Town Urban Design Plan for this Area.
4. Only Commercial Uses shall be located on the first (ground) floor in the areas identified on Appendix I.
5. The maximum Floor Area Ratio for Lots within Area F shall be 2.0.
6. The maximum Height for mid rise apartments shall not exceed 5 Storeys nor 18.0 m.
7. The maximum Height for buildings adjacent to Fort Road shall not exceed 5 Storeys nor 18 m.
8. All mechanical equipment, including roof mechanical units, shall be concealed by screening in a manner compatible with the architectural character of the building or concealed by incorporating it within the building and is not included in building height.
9. A 2.0 m building Setback shall be provided at the Rear of the building. Landscaping shall be provided within the 2.0 m building Setback to provide an improved appearance of the Lane.
10. A Minimum Private Outdoor Amenity Area of 7.5 m² per Dwelling for Dwellings no part of which is contained in the lowest Storey shall be provided.
11. The second floor in the areas identified for Commercial Uses, as shown on Appendix I, may be used exclusively for either Residential Uses or Commercial Uses, excluding Restaurants, Bars and Neighbourhood Pubs.
12. All commercial and commercial related uses shall not be developed above the second storey of the building and shall be located as shown in the Appendix I.
13. A minimum of 3 storeys residential development shall be provided above any commercial development.
14. Apartment Housing with Commercial Uses on the ground floor shall have access at Grade that is separate from the Commercial premises.
15. Landscaping shall contain at least 10% more trees and shrubs within any Yard or Setback than are required under Section 55 of the Zoning Bylaw.
16. The landscape plan required through Section 55.34 of the Zoning Bylaw, shall include landscaping as it relates to the design of special feature areas identified in the Urban Design Plan. Outdoor amenity spaces shall be landscaped for the purpose of achieving pedestrian connectivity, opportunities for play and social interaction. Furthermore outdoor amenity spaces should create a sense of place, character and identity. In addition to landscaping, this shall be achieved through the use of park benches, pedestrian level lighting, waste receptacles and other means integrated with the landscaping and placed along public walking routes through the site to the satisfaction of the Development Officer ~~in consultation with the Parks Branch at Asset Management and Public Works.~~
17. Loading, storage, and trash collection areas shall be located in such a manner to be screened from view from adjacent sites, public roadways in accordance with the provisions of Section 55 of the Zoning Bylaw. If the Rear or Sides of a Site are used for surface vehicular parking, it shall be screened in accordance with the provisions of Section 55 of the Zoning Bylaw.

18. ~~Vehicular Access shall be to a lane where one is available. vehicular Parking Garages will be from the internal roadway system, n~~ No direct access to parking will be permitted from Fort Road.
19. All residential parking shall be underground or in a structure, visitor and customer parking may be provided at grade.
20. Driveway ramps for underground parkades must not exceed a slope of 6% for the first 4.5 m from the property line and the ramp must be at Grade at the property line, to the satisfaction of the Transportation Department.
21. ~~Vehicular Parking on the site shall be developed in accordance with Section 54, Schedule 1 of the Edmonton Zoning Bylaw, except that:~~
 - a. ~~For Professional, Financial and Office Support Services at grade, parking shall be provided on the basis of 1.1 parking spaces per 100 m² of Floor Area and no parking spaces shall be required for this Use on upper floors;~~
 - b. ~~for Specialty Food Services, Restaurants, and Bars and Neighbourhood Pubs, parking shall be provided on the basis of one parking space per 4.8 m² of Public Space;~~
 - c. ~~for all other Commercial Use Classes, parking shall be provided on the basis of 1.1 parking spaces per 100 m² of Floor Area;~~
 - d. ~~For all Residential Use Classes, parking shall be provided on the basis of a minimum of 1 parking space per residential unit;~~
 - e. ~~a. The parking requirements may be further relaxed, at the discretion of the Development Officer in consultation with the Transportation Department, should initiatives which reduce parking demand be implemented. Such initiatives include but are not limited to dedicated car share/car pool stalls, universal bus pass programmes, or shared parking between commercial, commercial related, residential, and residential related uses; and~~
- f. Vehicular parking shall be located at the rear of the building.
22. ~~Bicycle Parking spaces shall be provided in accordance with Section 54 of the Edmonton Zoning Bylaw.~~
23. The owner shall submit a Crime Prevention Through Environmental Design (CPTED) Assessment that shall be reviewed and accepted by the Development Officer prior to the issuance of a Development Permit to ensure that development on the Site provides a safe urban environment in accordance with the guidelines and principles established in the Design Guide for a Safer City.
24. Signs in the areas identified for Commercial Uses shall comply with Schedule 59E of the Zoning Bylaw, with the intent to complement the pedestrian oriented commercial environment, except that:
 - a. a maximum Height of a Temporary Freestanding Sign shall be 6.0 m;
 - b. a Projecting Sign may be used to identify businesses that are located entirely at the second Storey level; and
 - c. the top Projecting Sign on a building two Storeys or higher shall not extend more than 75 cm above the floor of the second or third storey nor higher than the windowsill level of the second or third Story.
25. In the areas identified for Commercial uses, a Comprehensive Sign Plan and Schedule, consistent with the overall intent of subsection 59.3 of the Zoning Bylaw, shall be

prepared for the development and submitted, with the Development Application, to be approved by the Development Officer.

5. Urban Design Regulations

Site Planning

- a. Surface parking for visitors and customers shall be accessed off the lane, located to the rear of each building fronting onto Fort Road and screened from the street by appropriate orientation of the built form and enhanced landscaping measures.
- b. In the case of future redevelopment of the site no surface vehicular parking areas are permitted abutting any public roadway, other than a Lane.

Street Wall

- a. The maximum Frontage for Sites abutting a commercial shopping street shall be 10.06 m or consistent with the Frontage of other existing Sites abutting the shopping street and, where the Frontage for Sites abutting the commercial shopping street exceeds 10.06 m or the consistent development pattern for the street, the front façade of the building shall be designed to break the appearance into 10.06 m sections or modules consistently sized with other buildings on the shopping street.
- b. Blank walls (with or without windows) of vehicular Parking Garages shall not be developed adjacent to, or visible from, any public roadway.

Mid Rise Apartments

- a. Buildings shall address all adjacent public roadways, other than Lanes, with individual entrances that are clearly visible to lend a sense of occupancy to the street. Buildings on corner Sites shall address both the street and avenue and shall distinguish the street intersection to give it prominence.

Building Setbacks

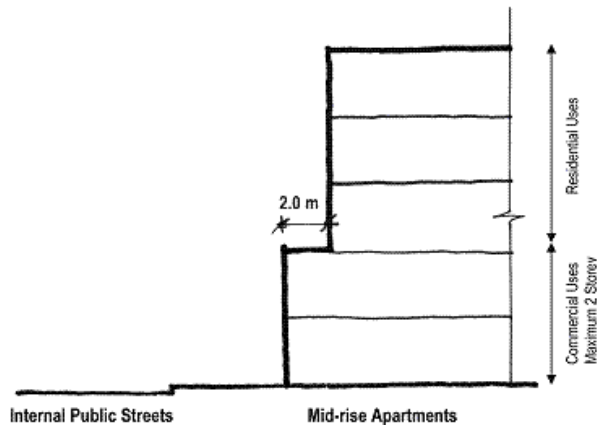
- a. Where the ground floor of any development is designed for Commercial Uses, buildings shall be built to the front and side property lines. The Development Officer may allow a maximum of 50% of the building to be setback by 1 m from the property line. This shall be done to create visual interest in the building. The Development Officer may allow an additional building setback, only to accommodate street related activities such as sidewalk cafes, colonnades, arcades or plazas.

Building Stepbacks

- a. Where Residential Uses are located above Commercial Uses in a building, the residential portion of the building above the second floor shall be setback a minimum 3 m from the lower portion of the building adjacent to Fort Road, as illustrated in the following diagram for interpretive purposes.



- b. Where Residential Uses are located above Commercial Uses in a building, the residential portion of the building above the second floor shall be setback a minimum 2 m from the lower portion of the building adjacent to a public roadway, other than a Lane, as illustrated in the following diagram for interpretive purposes.



- c. All developments shall provide a minimum 7.5 m building Setback from the Rear property line above the podium level of the building.
- d. The ground floor of any commercial development shall have a minimum of 50% of its floor space dedicated to commercial activity where all activity must abut the street frontages and be a depth of at least 10m.
- e. If parking uses front on the public street in any portion of the podium above the first and/or second storey level of the mid-rise apartment building then:
- i. An enhanced architectural façade treatment, harmonious with the adjacent commercial or residential frontage, shall be provided on the podium portion to the

- satisfaction of the development officer in consultation with the Urban Design Group;
and
- ii. Parking areas above second storey of the podium shall not be visible from the public street.

Entrances

- a. Main building entrances for any Use shall be designed for universal accessibility. Level changes from the sidewalk to entrances of buildings shall be minimized. Sidewalk furniture and other elements shall be located out of the travel path to ensure they are not obstacles to building access.
- b. The first floor of any residential frontage, including associated entranceway, shall have a maximum grade separation of 1 m from any adjacent public sidewalk.
- c. The first floor of commercial frontage and entranceways shall be at grade with the property line.
- d. A separate and prominent access to residential buildings from Fort Road is required for any building whose podium portion fronts onto Fort Road.

Weather Protection

- a. Where Commercial Uses are developed on the ground (first) floor of a building, weather protection in the form of a canopy at least 2.0 m wide or any other method suitable to the architectural style of the building or street theme, shall be provided one Storey above sidewalk level to provide a comfortable environment for pedestrians.

Architectural Control

- a. The finishing of the podium portion of any development shall consist of a combination of materials such as glass and glazed window wall systems, brick, stone, architectural concrete, and/or pre-cast coloured concrete.
- b. The use of stucco as a finishing material is not permitted on the podium portion of any development.
- c. Special architectural treatment of building faces shall be provided as per Appendix I.
- d. Where a Commercial Use is provided at ground level abutting a public roadway, other than a Lane, the principal entrance to the unit shall have direct external access to the adjacent public sidewalk. A minimum of 70% of the ground (first) floor level portion of the façade abutting a public roadway, other than a Lane, shall be comprised of clear, non-reflective glazing to promote pedestrian interaction and safety.
- e. Development on a Site shall incorporate functional and decorative lighting to highlight the building's architectural features and enhance the appearance of the building during the winter months.

6. Development Guidelines

- a. The developer is encouraged to incorporate a minimum of 10% adaptable dwelling units and to ensure universal accessibility.
- b. All building facades should use compatible and harmonious exterior finishing materials.
- c. Where feasible, developments should provide gardens or patios on the top of the podium level and building rooftops to improve rooftop aesthetics and provide additional amenity space.

- d. Where feasible, Dwellings and other elements of the development should be sited and oriented to minimize their impact on other Dwellings considering such things as daylight, sunlight, ventilation, quiet, visual privacy and views.
- e. Development in this Area should, to the greatest extent possible, utilize infrastructure practices, site designs and building developments that reduce the consumption of water, energy, and materials consistent with sustainability programs such as Built Green Alberta or Leadership in Energy and Environmental Design (LEED) accreditation.
- f. Landscaping of Sites in this Area should consider the use of plant materials that provide colour throughout the year to enhance their appearance during the winter months.
- g. Parking, loading and passenger drop-off areas should be easily accessible and designed to minimize pedestrian-vehicle conflicts.

Historic Resources

- a. The identified structures in Appendix II, should be retained and incorporated into any redevelopment, so that their historic façades and those elements of merit in the first 5 m become an integral part of the development, to the satisfaction of the Development Officer in consultation with the Heritage Officer.
- b. Any development behind or above should be set back from the principal façade of the identified historic structures and shall be complimentary in its design.
- c. Any redevelopment of the Transit Hotel should be to the rear of the original structure, such that the original structure be restored and maintained in appearance.

| [Appendix I](#)

| [APPENDIX II](#)

APPLICATION SUMMARY

INFORMATION

Application Type:	Rezoning
Charter Bylaw:	19936
Location:	North of Fort Road NW and east of 66 Street NW
Addresses:	12720, 12732, 12740, 12742, 12744, 12748, 12754, 12758, 12804 , 12812 , 12816, 12816A, 12834, and 12844, 12866 and 12890 - Fort Road NW, & 12820 - 64 Street NW
Legal Descriptions:	Lots 14-24, Block 7, Plan 2552S Lots 30-39, Block 2, Plan 2552S Lots 2-9, Block 3, Plan 3600U
Site Area:	CB2 - Two properties 0.1 ha total Revised DC1 Area - 15 properties of varying sizes
Neighbourhood:	Belvedere
Ward:	Dene
Notified Community Organizations:	Balwin Community League Belvedere Community League Edmonton North District Area Council Fort Road and Area Business Improvement Area
Applicant:	EINS Consulting Ltd.

PLANNING FRAMEWORK

Current Zone:	Direct Development Control Provision (DC1.14653)
Proposed Zones:	Direct Development Control Provision (DC1) General Business Zone (CB2)
Overlay:	Main Street Overlay
Plans in Effect:	Belvedere Station Area Redevelopment Plan Fort Road Old Town Master Plan Fort Road Old Town Master Plan Implementation Report
Historic Status:	Five properties within the revised boundary of the DC1 are on the City of Edmonton's Inventory of Historic Resources

Written By: Cyndie Prpich
 Approved By: Tim Ford
 Branch: Development Services
 Section: Planning Coordination