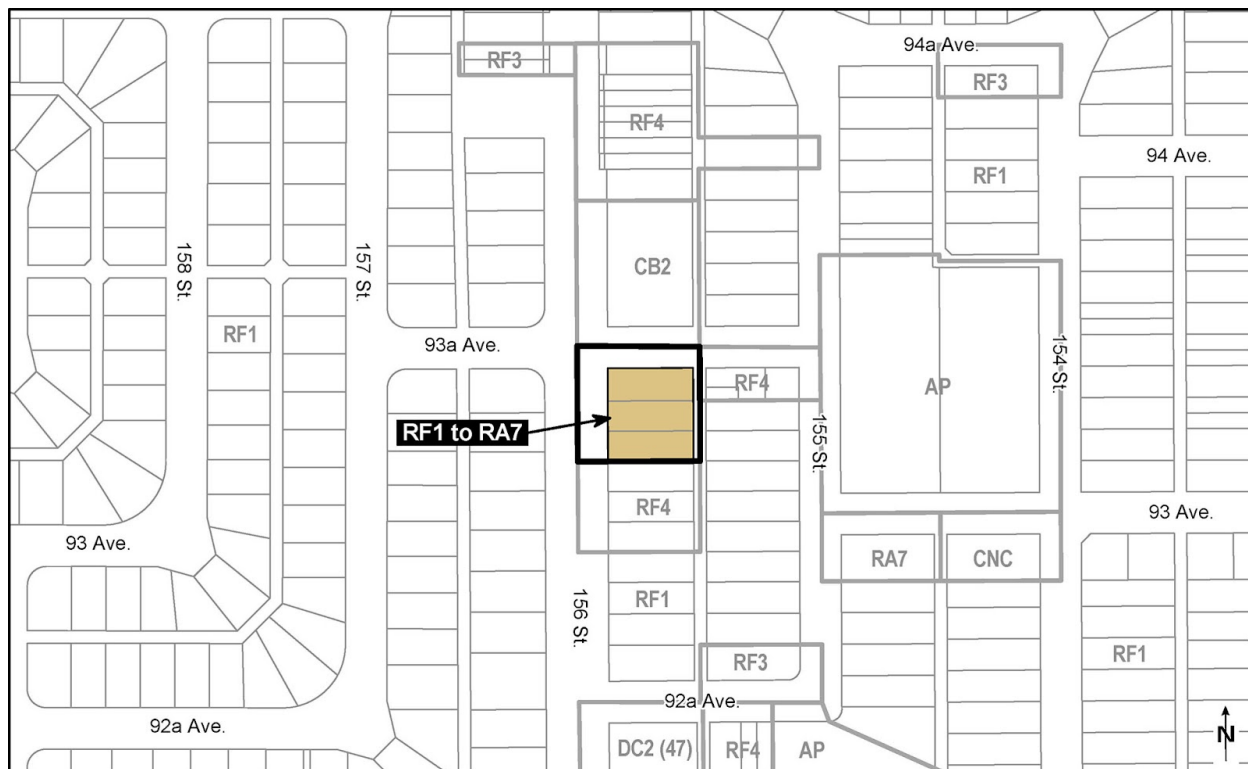


Administration Report Sherwood

Edmonton

9321, 9313 & 9311 – 156 Street NW

To allow for low rise multi-unit housing.



Recommendation: That Charter Bylaw 20297 to amend the Zoning Bylaw from (RF1) Single Detached Residential Zone to (RA7) Low Rise Apartment Zone be **APPROVED**.

Administration **SUPPORTS** this application because it:

- Provides the opportunity for increased density within walking distance of the future 156 Street/95 Avenue LRT stop;
- Is located at the edge of the neighbourhood where low rise apartment is an appropriate and compatible form of development; and
- Aligns with the objectives of The City Plan to intensify secondary corridors, and provides housing diversity in the Sherwood neighbourhood.

Application Summary

CHARTER BYLAW 20297 will amend the Zoning Bylaw, as it applies to the subject site, from (RF1) Single Detached Residential Zone to (RA7) Low Rise Apartment Zone for the purpose of allowing low-rise residential buildings up to 16 metres in height with limited commercial opportunities at the ground level.

This application was submitted by Tony Mahey on July 4, 2022.

Multi-unit Housing, up to 16 metres (approximately 4 storeys), with limited commercial opportunities at ground level, is a permitted use in the RA7 Zone and is the key difference between the existing RF1 Zone and the proposed RA7 Zone. The scale and uses of the proposed RA7 Zone are generally considered compatible with lower-intensity residential forms, and specific regulations in the RA7 Zone help to ensure this. The proposed RA7 Zone conforms to policy direction for increasing density at secondary corridors in both The City Plan and the Transit Oriented Development Guidelines. The proposed rezoning also aligns with the infill objectives of The City Plan to achieve 50 percent of new net units through infill development.

Community Insights

Based on the characteristics of this application, the file was brought forward to the public using the Broadened Approach. This approach was selected because the application proposes a standard zone that allows for an increase in height and density and is consistent with the objectives of The City Plan.

The Broadened Approach included the following techniques:

Advance Notice, July 27, 2022

- Number of recipients: 74
- Number of responses with concerns: 4

Webpage

- edmonton.ca/sherwoodplanningapplications

Common comments heard throughout the various methods include:

- Additional density will increase the crime in the area.
- The adjacent property owners will lose privacy and skyline view.
- Trees would need to be cut down to facilitate the proposed development.
- The proposed development will have impacts on existing sewage infrastructure and increase traffic congestion in the area.
- Uncertainty regarding access to the site.

Site and Surrounding Area

The subject properties are approximately 1869 m² in area, and located at the intersection of 93A Avenue and 156 Street NW at the edge of the Sherwood neighbourhood. The properties abuts 156 Street (secondary corridor) on the west, 93A Avenue on the north and the alley on the east. The subject property

is surrounded on all sides by single detached housing, with the exception of the north side of the property, which is bounded by multiple commercial uses.

The site is well connected to alternative modes of transportation such as bus service along 95 Avenue, and a bike lane along 153 Street (north of 95 Avenue). The future 156 Street/95 Avenue LRT stop on the Valley Line West LRT is within walking distance of the subject property.



Aerial view of application area

	EXISTING ZONING	CURRENT USE
SUBJECT SITE	(RF1) Single Detached Residential Zone	Single Detached House
CONTEXT		
North	(CB2) General Business Zone	Commercial Building
East	(RF1) Single Detached Residential Zone	Single Detached House
	(RF4) Semi-detached Residential Zone	Single Detached House
South	(RF4) Semi-detached Residential Zone	Single Detached House
West	(RF1) Single Detached Residential Zone	Single Detached House



View of the sites looking east from 156 Street NW



View of the sites looking southeast west from 93A Avenue and 156 Street NW

Planning Analysis

The City Plan

Within The City Plan, 156 Street is identified as a Secondary Corridor. Secondary corridors allow for an increase in density, achieved through low and mid-rise buildings and supported by city-wide mass transit. The proposed rezoning facilitates the intensification of this corridor and will contribute to the direction outlined in The City Plan to achieve 50 percent of new units through infill. Moreover, it will provide a wider variety of housing options in the area with convenient access to amenities and alternative modes of transportation.

Land Use Compatibility

The subject properties are surrounded on three of its four sides by road right-of-way. These roadways act as a buffer, helping to reduce the impacts of redevelopment. The key difference between the existing RF1 Zone and the proposed RA7 Zone is the introduction of Multi-unit Housing with a possibility of commercial use at ground level as a permitted use. The RA7 Zone allows for an additional 7.1 metres in height and an increase in density from what is permitted under the RF1 Zone.

The scale of the proposed RA7 Zone is compatible with lower intensity residential forms such as single detached houses in the RF1 Zone (see comparison table and graphics below), and it is common for these two forms to exist adjacent to each other in Edmonton, particularly along important corridors. However, to further improve the transition between the two, the side setback requirements in the RA7 Zone were amended by the City Council on December 7, 2021. The amendments increased the interior side setback from 1.2 metres to 1.5 metres when a proposed RA7 building is less than 10.0 metres in height and to 3.0 metres if the proposed building is more than 10.0 metres in height.

The below table is a comparison between key development regulations of each zone.

RF1 & RA7 Comparison Summary

	RF1 + MNO Current	RA7 Proposed
Principal Building	Single Detached Housing Duplex Housing Semi-detached Housing	Multi-Unit Housing
Maximum Height	8.9 m	14.5 m flat roof 16.0 m pitched roof
Minimum Front Setback (156 Street)	6.5	6.0 m
Minimum Interior Side Setback	1.2 m	3.0 m
Minimum Flanking Side Setback (93A Avenue)	1.2 m	3.0 m
Minimum Rear Setback (Lane)	16.58 m (40% of Site Depth)	7.5 m
Maximum Site Coverage	40% ¹	No Maximum

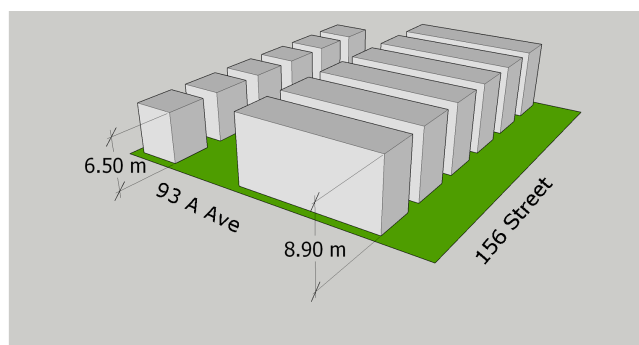
¹ A maximum Site Coverage of 40% would be applicable to each subdivided lot
Attachment 2 | File: LDA22-0318 | Sherwood

Maximum Floor Area Ratio (FAR)	n/a	2.5 FAR
Number of Dwelling Units	Min: n/a Max: Six (6) Principal Dwellings ²	Min: 45 dwellings/hectare Max: n/a

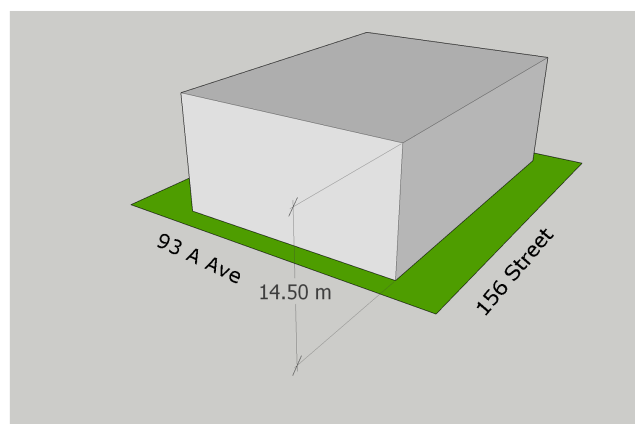
	RF1 + MNO: Current	
Accessory Building	Garden Suite	Detached Garage
Maximum Height	6.5 m	4.3 m
Minimum Interior Side Setback	1.2 m	0.6 m
Minimum Flanking Side Setback	1.2 m	1.2 m
Minimum Rear Setback	1.2 m	1.2 m

POTENTIAL RF1 BUILT FORM

Top view

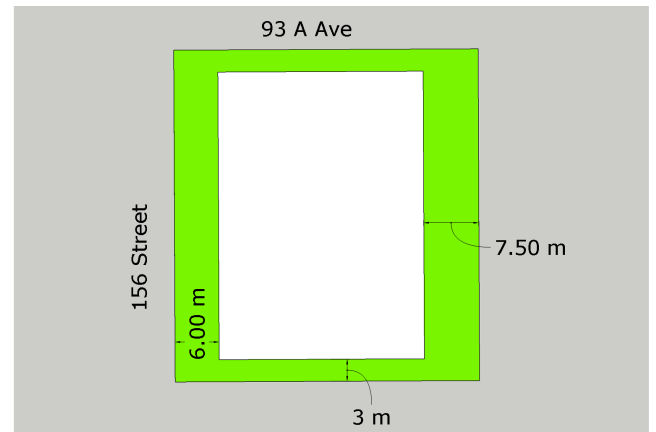
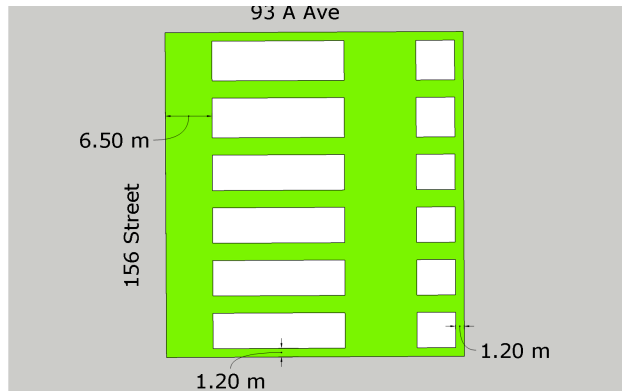


POTENTIAL RA7 BUILT FORM



² Lot Subdivision would be required to accommodate two Single Detached principal structures/dwellings per existing lot under RF1. Semi-detached could be built without subdividing under RF1. Each principal dwelling could have a Secondary Suite and/or Garden Suite.

3D view



Transit Oriented Development Guidelines

This site is located within 300 metres of the future 156 Street/95 Avenue LRT stop. This stop is identified within the Transit Oriented Development Guidelines as a Neighbourhood Stop. The TOD guidelines seek to ensure that the larger sites within a 400-metre radius of an LRT station provide an opportunity for more transit-supportive densities.

The TOD guidelines also include a recommendation that new low-rise buildings next to existing houses should be designed to create a gradual transition in height away from the existing house using stepbacks. The recently amended RA7 Zone increased the required setback from 1.2 to 3.0 metres but took out the requirement for a stepback. This manages building transitions and reduces the overall massing and impact of the building while allowing for increased landscaping viability within the setback to help address concerns about privacy and overlook. It is concluded that the intent around transitions in these guidelines is still being met by the recently revised version of the RA7 Zone through an increased overall setback.

Technical Review

Transportation

Vehicular access shall be from the adjacent lane to conform with the Zoning Bylaw. The owner will be required to upgrade the lane adjacent to the site to a commercial alley standard and construct a sidewalk on the south of 93A Avenue adjacent to the site.

Drainage

Development allowed under the proposed zone would be required to include on-site stormwater management techniques utilizing a controlled outflow rate to mitigate its impact on the existing drainage infrastructure. Details of the required on-site stormwater management will be reviewed at the Development Permit stage.

EPCOR Water

Water service is available to this site from an existing 200mm water main on 156 street.

The applicant/owner will be responsible for all costs associated with providing City standards for water supply, including any changes to the existing water infrastructure required by this application.

All other comments from affected City Departments and utility agencies have been addressed.

Appendices

1 Application Summary

Application Summary

Information

Application Type:	Rezoning
Charter Bylaw:	20297
Location:	south of 93A Avenue NW and east of 156 Street NW
Address(es):	9321, 9313, 9311 - 156 Street NW
Legal Description(s):	Lots 12 to 14, Block 9, Plan 3963HW
Site Area:	1869 m ²
Neighbourhood:	Sherwood
Ward:	Sipiwiyiniwak
Notified Community Organization:	West Jasper - Sherwood Community League
Applicant:	Tony Mahey

Planning Framework

Current Zone and Overlay:	(RF1) Single Detached Residential Zone (MNO) Mature Neighbourhood Overlay (MSO) Main Street Overlay
Proposed Zone and Overlay:	(RA7) Low Rise Apartment Zone
Plan in Effect:	None
Historic Status:	None

Written By:	Abhimanyu Jamwal
Approved By:	Tim Ford
Branch:	Development Services
Section:	Planning Coordination