

Charter Bylaw 20298

A Bylaw to amend Bylaw 12800, as amended,  
The Edmonton Zoning Bylaw  
Amendment No. 3548

WHEREAS Lot 22, Block 42, Plan 7722150; located at 12719 - Victoria Trail NW, Canon Ridge, Edmonton, Alberta, is specified on the Zoning Map as (CNC) Neighbourhood Convenience Commercial Zone; and

WHEREAS an application was made to rezone the above described property to (DC2) Site Specific Development Control Provision;

NOW THEREFORE after due compliance with the relevant provisions of the Municipal Government Act RSA 2000, ch. M-26, as amended, the Municipal Council of the City of Edmonton duly assembled enacts as follows:

1. The Zoning Map, being Part III to Bylaw 12800 The Edmonton Zoning Bylaw is hereby amended by rezoning the lands legally described as Lot 22, Block 42, Plan 7722150; located at 12719 - Victoria Trail NW, Canon Ridge, Edmonton, Alberta, which lands are shown on the sketch plan annexed hereto as Schedule "A", from (CNC) Neighbourhood Convenience Commercial Zone to (DC2) Site Specific Development Control Provision.
2. The uses and regulations of the aforementioned DC2 Provision are annexed hereto as Schedule "B".

3. The sketch plan annexed hereto as Schedule "A" and the uses and regulations of the DC2 Provision shown on Schedule "B" annexed hereto are hereby incorporated into the Zoning Bylaw, being Part IV to Bylaw 12800, The Edmonton Zoning Bylaw.

READ a first time this	day of	, A. D. 2022;
READ a second time this	day of	, A. D. 2022;
READ a third time this	day of	, A. D. 2022;
SIGNED and PASSED this	day of	, A. D. 2022.

THE CITY OF EDMONTON

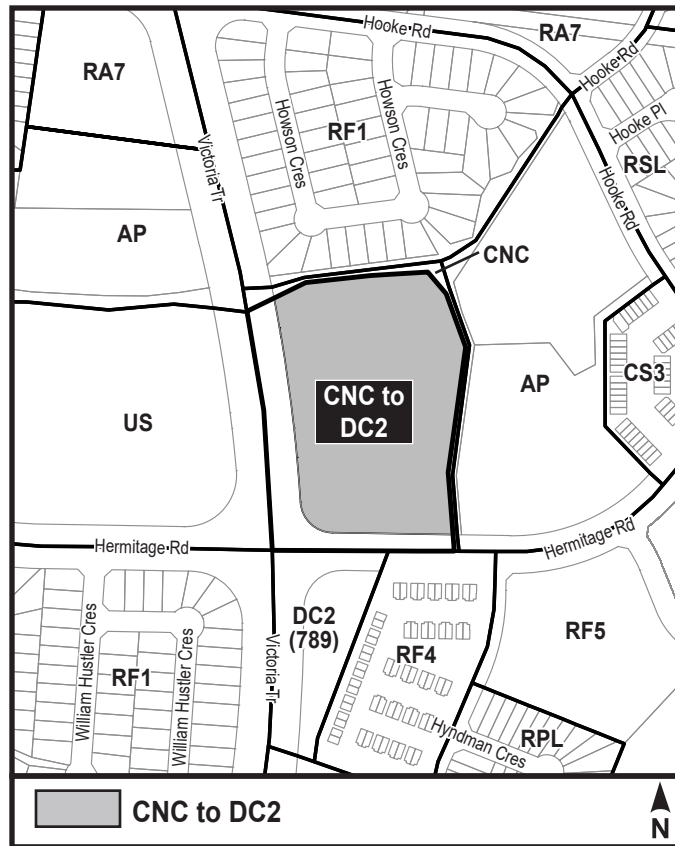
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MAYOR

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CITY CLERK

## CHARTER BYLAW 20298



**(DC2) SITE SPECIFIC DEVELOPMENT CONTROL PROVISION  
CANON RIDGE**

**1. General Purpose**

The purpose of this Zone is to provide for multi-unit housing developments including row housing and mid-rise apartment housing, and standalone commercial development along Victoria Trail NW to appropriately transition with adjacent sites.

**2. Area of Application**

This Provision shall apply to Lot 22, Block 42, Plan 7722150, located at 12719 - Victoria Trail NW, as shown on Schedule "A" of the Charter Bylaw adopting this Provision, Canon Ridge.

**3. Uses**

- a. Automotive and Equipment Repair Shops
- b. Bars and Neighbourhood Pubs
- c. Breweries, Wineries and Distilleries
- d. Business Support Services
- e. Cannabis Retail Sales
- f. Child Care Services
- g. Commercial Schools
- h. Convenience Retail Stores
- i. Creation and Production Establishments
- j. Drive-in Food Services
- k. Gas Bars
- l. General Retail Stores
- m. Health Services
- n. Indoor Participant Recreation Services
- o. Liquor Stores
- p. Live Work Units
- q. Major Amusement Establishments
- r. Major Home Based Business
- s. Minor Amusement Establishments
- t. Minor Home Based Business
- u. Minor Service Stations
- v. Mobile Catering Food Services
- w. Multi-unit Housing
- x. Personal Service Shops
- y. Professional, Financial and Office Support Services
- z. Rapid Drive-through Vehicle Services

- aa. Residential Sales Centre
- bb. Restaurants
- cc. Specialty Food Services
- dd. Urban Gardens
- ee. Urban Indoor Farms
- ff. Urban Outdoor Farms
- gg. Veterinary Services
- hh. Fascia Off-premises Signs
- ii. Fascia On-premises Signs
- jj. Freestanding Off-premises Signs
- kk. Freestanding On-premises Signs
- ll. Major Digital Signs
- mm. Minor Digital Off-premises Signs
- nn. Minor Digital On-premises Signs
- oo. Minor Digital On-premises Off-premises Signs
- pp. Projecting On-premises Signs
- qq. Roof On-premises Signs
- rr. Temporary Off-premises Signs
- ss. Temporary On-premises Signs

#### **4. Development Regulations for Uses**

- a. Development shall be in general accordance with Appendix I.
- b. The maximum Floor Area Ratio shall be 1.0 for the Site.
- c. The maximum building Height shall not exceed:
  - i. 16.0 m for commercial buildings;
  - ii. 23.0 m for mid-rise apartment buildings; and
  - iii. 12.0 m for row housing.
- d. A Setback of 3.0 m shall be required along Victoria Trail and Hermitage Road.
- e. A Setback of 1.5 m shall be required along the north and east property line, except that:
  - iv. For buildings over 10.0 m in Height, the Setback shall be 3.0 m.
  - v. A Setback of 0.0 m shall be required along internal property lines within the DC2.
- f. Trash collection for commercial development shall not require screening if moloks are installed.
- g. Trash collection for Residential Uses may be permitted within a Setback, provided there is a minimum 1.5 m wide landscaping buffer.
- h. The maximum Floor Area per business shall be 2,500 m<sup>2</sup>.
- i. A Crime Prevention Through Environmental Design (CPTED) assessment shall be submitted with the development permit application to ensure that the development provides a safe urban environment and to show that the development meets the requirements of Section 58 of the Zoning Bylaw, to the satisfaction of the Development Officer.

- j. Barrier-free design principles shall be used to guide the design of all buildings and public facilities, ensuring a high degree of accessibility to persons with disabilities.
- k. All mechanical equipment, including roof mechanical units shall be minimized from view in a manner compatible with the architectural character of the building or concealed by incorporating it within the building.
- l. The following regulations shall apply to Rapid Drive-through Vehicle Services developments:
  - i. the total number of bays shall not exceed four for any given Site; and
  - ii. all mechanical equipment associated with this Use shall be located within an enclosed building.
- m. Signs shall comply with the regulations found in:
  - i. Schedule 59B for residential uses, and
  - ii. Schedule 59E for commercial uses.
- n. Signs shall be designed in an aesthetically pleasing manner and in harmony with the architecture theme of the respective building.
- o. Directional signage is encouraged to provide information at critical locations relating to primary tenants, parking lots, loading zones and pick up areas, entrances, and exits.

## **5. Development Regulations for Commercial Building Design and Features**

- a. Building façades shall include design elements, low maintenance and high quality finishing materials, and variations that will reduce perceived mass and linearity of buildings and add architectural interest.
- b. Buildings shall incorporate decorative exterior lighting to enhance building architecture
- c. The massing of building walls greater than 20.0 m in length shall be reduced through architectural elements such as columns, ribs, pilasters or piers, changes in plane (e.g., recesses and projections), changes in building finishes, materials and textures, or features that create an identifiable pattern and sense of human scale.
- d. Side, front, and rear Façades shall include design techniques including, but not limited to, the use of varied rooflines, variations in building Setbacks and articulation of building Façades, in order to minimize the perception of massing, eliminate large uninterrupted expanses of wall, and provide visual interest when the structure is viewed from an adjacent Lot or roadway.

## **6. Development Regulations for Residential Building Design and Features**

- a. All principal Dwellings in apartment buildings shall have an individual entrance off an internal hallway.
- b. Main floor patios at grade should be constructed with privacy railings and landscaped screens, and face public spaces (Commercial developments to the

west, and Row Housing developments to the east) providing passive surveillance.

- c. Perceived massing shall be minimized by the following elements: building façade incorporating setback variations, building orientation, window placement, articulation around entranceways, roof treatment, and the choice of exterior materials and colors.

## **7. Development Regulations for Pedestrian Connections**

- a. Enhanced pedestrian connections linking the Commercial development to the Residential development shall be provided to facilitate safe pedestrian movement, in accordance with Appendix I. This may include pedestrian-scale lighting, street furniture, landscaping, and/or other elements.



*Figure 1: Example of pedestrian-scale lighting.*

- b. Parking, loading, and passenger drop-off areas shall be designed to minimize pedestrian-vehicle conflicts and prioritize pedestrian movement through the Site using clearly demarcated Walkways, decorated painting, and signage systems to reduce pedestrian conflict with vehicles.
- c. Sidewalks in front of commercial main entrances shall be developed to a minimum 2.0 m width, to reduce pedestrian conflict and facilitate pedestrian movement and activities.

## **8. Development Regulations for Landscaping, Lighting, and Amenity Area**

- a. A Landscape Plan prepared by a registered landscape architect shall be submitted for review and approval prior to the issuance of any Development Permit. At the Development Permit Stage, a generalized overall landscape concept plan will be submitted for all new building development to illustrate how the required landscaping will integrate across the entire site. Landscaping requirements for the commercial and residential landscaping requirements can be located on either site. Each Landscape Plan will include landscaping internal to the Site, pedestrian linkages, and Amenity Areas, where applicable.

- b. Open spaces shall be landscaped for the purpose of achieving pedestrian connectivity, opportunities for play and social interaction. This may include benches, pedestrian-scale lighting, waste receptacles, and other means integrated with the landscaping and placed along public walking routes through the Site.
- c. Landscaping shall be used to highlight major circulation patterns, and pedestrian linkages.
- d. A minimum Amenity Area of 7.5 m<sup>2</sup> per Dwelling shall be provided.
- e. Amenity Areas may be developed as balconies, patios, courtyard, interior landscaped open space, or outdoor seating area. Amenity Areas may be developed with landscaping, planting beds, street furniture, or other enhancing features.

## **9. Development Regulations for Parking, Loading, Storage, and Access**

- a. Vehicular access shall include a right-in only access and an all directional access on Victoria Trail NW, and an all directional access along Hermitage Road NW, in accordance with Appendix 1.
  - i. Notwithstanding the above, the right-in only access to Victoria Trail NW may be upgraded to include a right-out movement if it can be demonstrated that the access location shall meet the City's Access Management Guidelines and be constructed to the satisfaction of Subdivision and Development Coordination (Transportation).
- b. No parking, loading, storage, outdoor service or display area shall be permitted within a Setback. Vehicular parking, loading, and storage areas shall include landscaping between adjacent Sites, and public roadways.
  - i. Notwithstanding the above, parking, loading, or storage for Residential Uses may be permitted within a Setback, provided a minimum 1.5 m wide landscaping buffer, is provided within the Setback and that all parking and loading areas are screened by a wood fence from any Site zoned to allow Single Detached Housing as a Permitted Use.
- c. All waste collection, storage, or loading areas shall be designed to the satisfaction of the Development Officer in consultation with Waste Management Services and Subdivision and Development Coordination (Transportation). Gates and/or doors of the waste enclosure shall not open or encroach into road right-of-way.

## **10. Off-Site Improvements**

- a. As a condition of any development permit, the Owner shall enter into an Agreement with the City of Edmonton for off-site improvements necessary to serve the development to the satisfaction of the Development Officer in consultation with Subdivision and Development Coordination (Transportation) and Traffic Operations. Such improvements shall be constructed at the Owner's cost. The agreement process shall include an engineering drawing review and



approval process. Improvements to be addressed in the Agreement include but are not limited to:

- i. Repair of any damage to the abutting roadways, sidewalks, and boulevard, including Lanes not directly adjacent to the Site caused by the construction of the development;
- ii. Removal of the existing access to Hermitage Road NW and construction of the centre median, boulevard, sidewalk, curb, and gutter as required;
- iii. Construction of new site accesses along Victoria Trail NW and Hermitage Road NW in accordance with Appendix 1;
- iv. A contribution of 50% of the cost towards pedestrian control at the intersection of Victoria Trail NW and the all-directional development site access;
- v. Adjustments to the traffic signals at the Hermitage Road NW and Victoria Trail NW intersection, including any new traffic control infrastructure necessary to add a westbound left turn phase to mitigate westbound queues; and
- vi. Improvements along the north side of Hermitage Road NW:
  - a) boulevard and/or shared use path that may extend onto the owner's property to enhance the pedestrian realm and bike network. Any portion of the public realm that is on Site shall be accessible to the public at all times through the registration of a 24-hour Public Access Easement in favour of the City of Edmonton, which shall be a condition of the development permit; and
  - b) other boulevard treatments specific to the roadway.

