

11563, 11565 & 11567 - University Avenue NW Position of Administration: Support



Summary

Charter Bylaw 20660 proposes a rezoning from the (RF1) Single Detached Residential Zone with the Mature Neighbourhood Overlay to the (RA7) Low Rise Apartment Zone to allow low rise multi-unit housing. Bylaw 20659 proposes an amendment to the McKernan-Belgravia Station Area Redevelopment Plan to facilitate the proposed rezoning.

Public engagement for this application included a mailed notice, site signage and information on the City's webpage. Six people were heard from, all in opposition, while the McKernan Community League expressed support. Most concerns were related to increased traffic issues and privacy impacts.

Administration supports this application because it:

- Increases residential density close to a major employment centre and near mass transit options.
- Provides a good transition between the University/Garneau Major Node and the residential neighbourhood to the south.

Application Details

This application was submitted by Green Space Alliance Consulting Inc. on behalf of the landowners.

Rezoning

The proposed (RA7) Low Rise Apartment Zone would allow development with the following key characteristics:

- A residential building with limited commercial opportunities at ground level, such as child care services, general retail stores and specialty food services.
- A height of up to 16 metres (approximately 4 storeys)

Plan Amendment

The proposed amendments to the McKernan-Belgravia Station Area Redevelopment Plan to facilitate the proposed rezoning are:

- Figure 15: Amending the rezoning site from “Small Scale Residential Infill” to “Residential (4 storey max).”
- Figure 16: Amending the rezoning site from being in the “Neighbourhood Infill” Precinct to being in the “University Avenue Corridor” Precinct.
- Figure 23: Amending the rezoning site to identify it for “Maximum 4 storeys”.
- Figure 24: Amending the rezoning site to identify it as containing “Existing buildings planned for low to medium scale redevelopment.”
- Policy 3 of Section 4.4.1: Amending to allow for a 4 storey height to be developed on the southwest corner of University Avenue NW and 115A Street NW.

Site and Surrounding Area

	Existing Zoning	Current Development
Subject Site	(RF1) Single Detached Residential Zone	Single Detached Housing
North	(AJ) Alternative Jurisdiction Zone	Cross Cancer Institute
East	(RF1) Single Detached Residential Zone	Single Detached Housing

South	(RF1) Single Detached Residential Zone	Single Detached Housing
West	(RF1) Single Detached Residential Zone	Semi-detached Housing



View of site looking southwest from University Avenue NW/115A Street NW Intersection



View of site looking southeast from University Avenue NW



View of site looking north from rear alley

Community Insights

This application was brought forward to the public using a basic approach. This approach was selected because there were few responses to the initial mailed notification. The basic approach included:

Mailed Notice, July 25, 2023

- Notification radius: 60 metres
- Recipients: 28
- Responses: 7
 - In support: 1 (14%)
 - In opposition: 6 (86%)

Site Signage, August 17, 2023

- Two rezoning information signs were placed on the site, one facing University Avenue NW and one facing 115A Street NW.

Webpage

- edmonton.ca/rezoningapplications

Notified Community Organizations

- Belgravia Community League
- McKernan Community League
- Windsor Park Community League
- Central Area Council of Community Leagues

Common comments heard (number of similar comments in brackets beside comments below):

- Traffic issues, including in alleys (4).
- Concerned about privacy and overlook from a multi-storey building to the houses across the alley (2).
- More density brings more noise, garbage and crime. Already not feeling safe anymore for families in this area (2).
- Will negatively impact the character of the neighbourhood.
- Not aligned with current direction from the ARP.
- Decline in property values will lead to more rental houses nearby as owners move out due to not wanting to be near the multi-storey building. Properties will decline in appearance.
- Concerned about increased street parking, especially if there are commercial uses.
- Other vacant sites in the City (Blatchford, Exhibition Lands) should be built with more intense forms of development before taking down houses in mature neighbourhoods.

The McKernan Community League has expressed support for this application citing a desire to see commercial uses incorporated into the new building in support of The City Plan and the new Zoning Bylaw. They also expressed a desire for the new building to have as little vehicular parking as possible to emphasize and support alternative modes of transportation.

In response to the concerns raised, Administration shared information about Open Option Parking, details from the Area Redevelopment Plan, and steps of the rezoning process, including how to provide feedback and how it is used.

Application Analysis



Site analysis context

The City Plan

The City Plan identifies the University-Garneau Major Node as one of six Major Nodes spread out across the City. While specific boundaries of these nodes have yet to be determined through the District Planning process, it is likely that this site would be considered at the edge of the Node. Seeing as Major Nodes are directed for mid and high rise development, a low rise development in this edge condition is seen to provide a transition away from the Node into the neighbourhood to the south.

Area Redevelopment Plan

The McKernan-Belgravia Station Area Redevelopment Plan divides the neighbourhoods into six Land Use Precincts, each with a set of policies to support different development outcomes. Although along University Avenue NW, this site is currently in the Neighbourhood Infill Precinct, with the University Avenue Corridor Precinct ending east of 115A Street NW. While low rise buildings are supported in the University Avenue Corridor Precinct mainly through the RA7 Zone, the plan only supports the RF1 Zone on this site with policies that it should be retained.

The proposed amendments with this application move the rezoning site from being in the Neighbourhood Infill Precinct to being within the University Avenue Corridor Precinct, essentially extending the allowance for low rise buildings further west down University Avenue NW. Generally, low rise buildings are considered appropriate on corner sites on the edge of the neighbourhood that front an arterial or service road, such as this. These amendments are acceptable as they help better align the ARP with The City Plan.

Impact of Zoning Bylaw Renewal

At the time of writing of this report, Zoning Bylaw 20001 has been approved but is not yet in effect. As a result, if proposed Charter Bylaw 20660 is approved, the RA7 Zone would be effective until December 31, 2023, while the RM h16 Zone would become effective January 1, 2024.

Land Use Compatibility

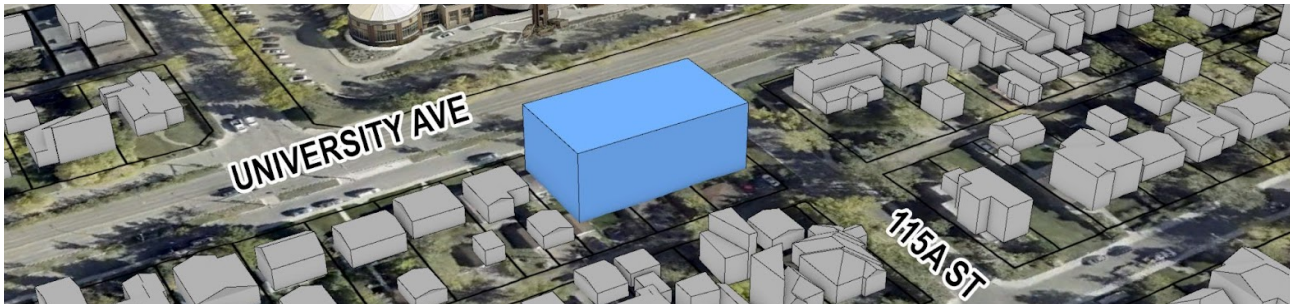
The proposed RA7 Zone is generally considered compatible with the surrounding RF1 Zoning. The site is separated from the RF1 zoned land by either a road or an alley, except to the west. The table below shows key differences between the current and proposed zone and also includes details of the RM h16 Zone, which would come into effect on January 1, 2024 for this site, if this rezoning is approved.

	RF1 + MNO Current	RA7 Proposed	RM h16 Zoning Bylaw 20001 Equivalent
Typical Uses	Single detached housing, semi-detached housing, secondary suites, garden suites	Multi-unit Housing, supportive Housing, child care services, specialty food services	Residential, home based business, child care service, food and drink service, indoor sales and service
Number of Dwellings	No min Max: 6 principal	Min: 9 (45 du/ha) No max	Min: 9 (45 du/ha) No max
Maximum Floor Area Ratio	N/A	2.3 - 2.5	2.3 - 3.0
Maximum Height	8.9 m	16.0 m	16.0 m
Minimum Front Setback (University Avenue NW)	5.2 m	5.2 m	4.5 m

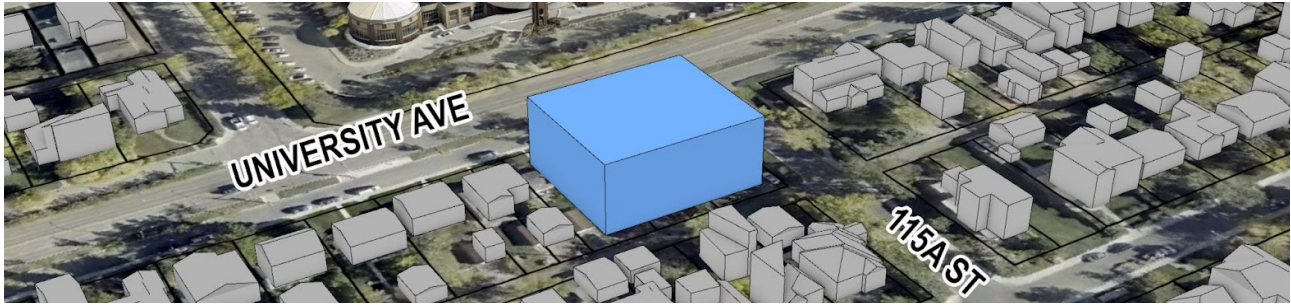
Minimum Interior Side Setback (West)	1.2 m	3.0 m	3.0 m
Minimum Flanking Side Setback (115A Street NW)	1.2 m	3.0 m	4.5 m
Minimum Rear Setback (Alley)	16.0 m	7.5 m	3.0 m

The main differences between the proposed RA7 Zone and the RM h16 Zone under Zoning Bylaw 20001 are that the minimum front setback would be reduced from 5.2 m to 4.5 m, the minimum flanking side setback would increase from 3.0 m to 4.5 m and the minimum rear setback would be reduced from 7.5 m to 3.0 m. The maximum floor area ratio is designed to not allow a building to be built to the full height and all the minimum setbacks, so the reduced setback requirement from an alley provides more development flexibility and the opportunity to provide a larger side setback from abutting sites.

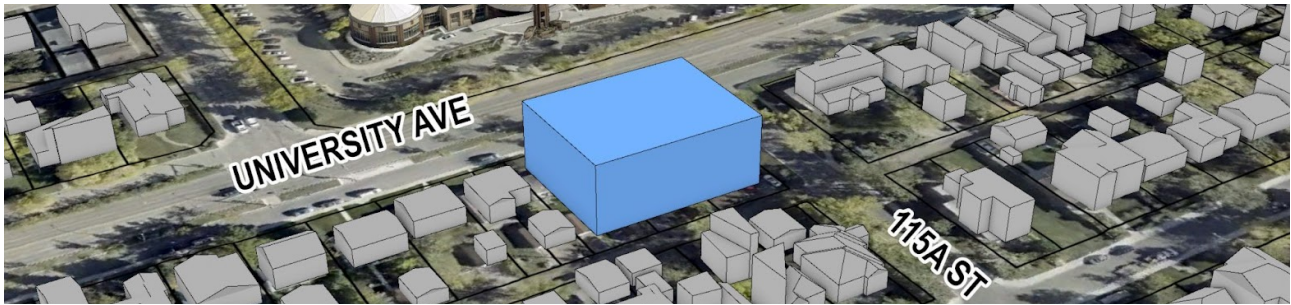
Below is a visual comparison of the proposed RA7 Zone under the current Zoning Bylaw and the RM h16 Zone under the proposed new Zoning Bylaw. Two scenarios are shown for the RM h16 Zone depending on how the developer chooses to design the building relative to floor area ratio and setbacks.



RA7 Massing



RM h16 Massing Scenario #1 - Minimum Rear Setback



RM h16 Massing Scenario #2 - Minimum Side Setbacks

Relative to concerns through engagement about privacy and overlook to the south, in addition to the rear setback being reduced, the RM h16 Zone does not give the Development Planner the power to ask for adjustments to the location of windows and amenity areas to minimize overlook into adjacent properties. In a growing and densifying city, it is likely that development over fence height will result in situations where there is overlook between properties and the new Zoning Bylaw 20001 does not prescriptively regulate locations of windows and amenity areas. That said, the required 3.0 m rear setback combined with the 6.0 m alley width is considered an acceptable separation distance between developments relative to privacy and overlook concerns.

Mobility

The site is located in close proximity to existing and planned active modes infrastructure, including a bike route on 115 Street NW and a future greenway on the University Avenue NW service road east of 115A Street NW. The construction of a pedestrian crossing on the west leg of the University Avenue NW and 114 Street NW intersection is scheduled for spring 2024.

Bus service is available along University Avenue NW and 117 Street NW, with the closest stop being directly across from the site on University Avenue NW. Additional bus service and LRT mass transit operate along 114 Street NW, approximately 500 - 700 metres walking distance to

the site. In addition to LRT mass transit, mass transit bus routes are anticipated to operate on 114 Street NW as part of the future mass transit network associated with the 1.25 million population scenario of The City Plan.

The construction of a new public sidewalk on the west side of 115A Street NW, between the University Avenue NW service road and the rear alley, will be required with redevelopment of the site. Vehicular access to the site will be via the adjacent alley. Requirements for upgrades to the alley will be reviewed at the Development Permit stage. Should the development include commercial uses, upgrades to a commercial standard alley may be required.

Utilities

Development allowed under the proposed zone would be required to include on-site stormwater management techniques utilizing an engineered outflow rate to mitigate its impact on the existing drainage infrastructure. Details of the required stormwater management will be reviewed at the Development Permit stage.

The applicant/owner will be responsible for all costs associated with providing required water supply including any changes to the existing water infrastructure required by the proposed zoning.

Appendices

1. ARP Development Concept Map Comparison

Written By: Andrew McLellan

Approved By: Tim Ford

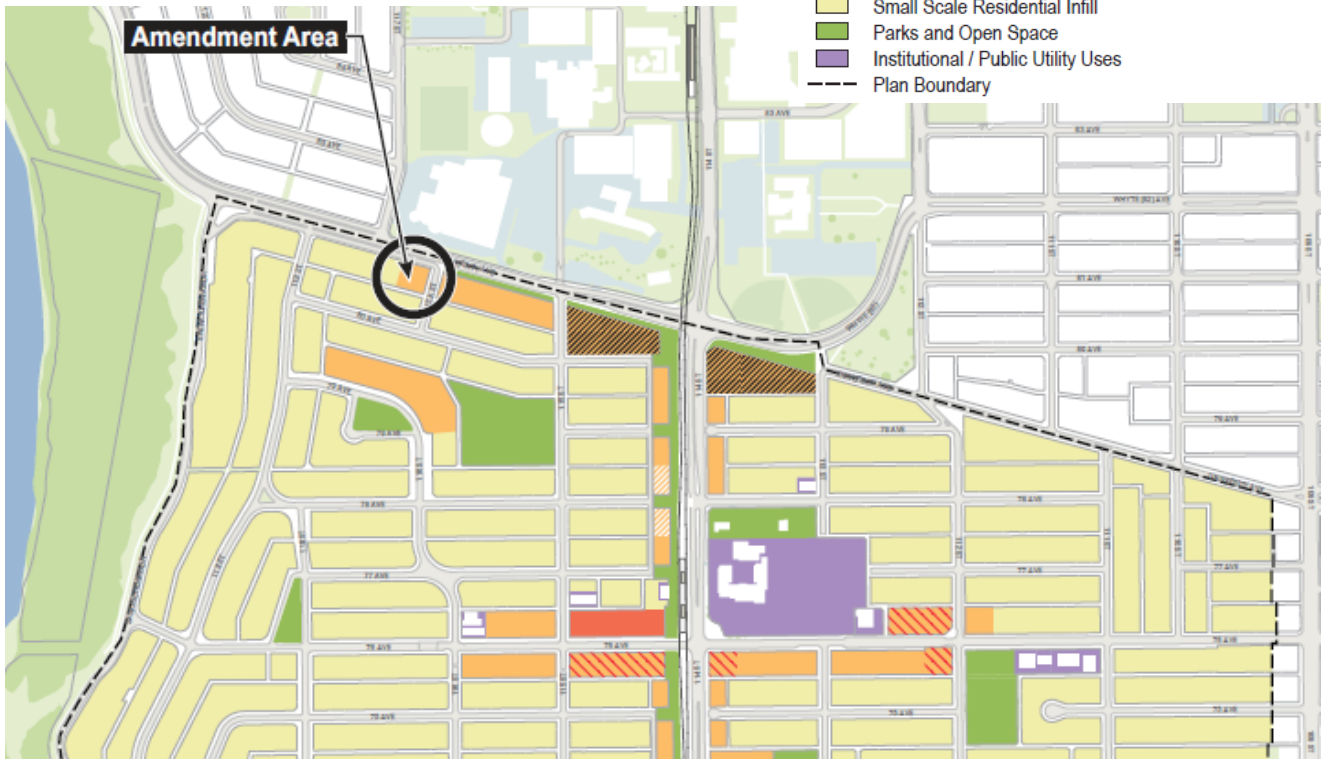
Branch: Development Services

Section: Planning Coordination

ARP Development Concept Map Comparison



Current Development Concept Map



Proposed Development Concept Map