

### 10903, 10907, 10909, 10913, 10917 and 10921 - University Avenue NW

#### Position of Administration: Support



## Summary

Bylaw 21181 proposes a rezoning from the Small Scale Residential Zone (RS) and Medium Scale Residential Zone (RM h16.0) to the Medium Scale Residential Zone (RM h23.0) to allow for medium scale housing. Public engagement for this application included a mailed notice, site signage and information on the City's webpage. Administration heard from 4 people with most concerns related to increased parking and traffic congestion and that the proposed scale is not in line with the neighbourhood character.

Administration supports this application because it:

- Will facilitate an increase in residential density in a redeveloping area of the City near existing transit, future bus rapid transit, and active modes of transportation.

- Proposes a scale that is aligned with direction in the Scona District Plan and District Policy for Secondary Corridors along arterial roadways.

## Application Details

This application was submitted by Situate Inc. on behalf of Greystone Group Ltd.

The proposed Medium Scale Residential Zone (RM h23.0) would allow development with the following key characteristics:

- Mid rise multi-unit residential development.
- A maximum height of 23.0 metres (approximately 6 storeys).
- Limited commercial opportunities at ground level.

## Site and Surrounding Area

	Existing Zoning	Current Development
<b>Subject Site</b>	Small Scale Residential Zone (RS) Medium Scale Residential Zone (RM h16.0)	Vacant and single detached housing
<b>North</b>	Small Scale Residential Zone (RS) Medium Scale Residential Zone (RM h16.0)	Variety of small scale housing, including multi-unit housing
<b>East</b>	Medium Scale Residential Zone (RM h16.0) Neighbourhood Parks and Services Zone (PSN)	Small scale multi-unit housing Joe Morris Park
<b>South</b>	Small Scale Residential Zone (RS) Medium Scale Residential Zone (RM h16.0)	Vacant and single detached housing
<b>West</b>	Small Scale Residential Zone (RS)	Single detached housing





*View of site looking southwest from the corner of 109 Street NW and University Avenue NW*



*View of site looking northeast from the alley*

## Community Insights

This application was brought forward to the public using a basic approach. This approach was selected because it proposes a rezoning to a standard zone of the same category in the Zoning



Bylaw, follows a statutory plan in place and raised little response to initial mailed notices. The basic approach included:

### **Mailed Notice, February 27, 2025**

- Notification radius: 60 metres
- Recipients: 154
- Responses: 4, all in opposition

### **Site Signage, March 21, 2025**

- One rezoning information sign was placed on the property so as to be visible from both 109 Street NW and University Avenue NW.

### **Webpage**

- [edmonton.ca/rezoningapplications](http://edmonton.ca/rezoningapplications)

### **Notified Community Organizations**

- McKernan Community League
- Queen Alexandra Community League
- Garneau Community League
- Scona District Community Council

### **Common comments heard (number of similar comments in brackets beside comments below):**

- Open Option Parking is a problem and will increase street parking congestion (x3).
- A larger building will bring more noise (x2).
- Worried about increased traffic, especially in the alley (x2).
- A building of this size is out of character with McKernan, there are not really any other buildings this tall in the neighbourhood (x2).
- 109 Street Corridor Area Redevelopment Plan went through proper engagement and should have been followed instead of rescinded.
- 4 storeys and 4 lots of depth should be the future here.
- The lots furthest from 109 Street should have to taper down in height.
- Traffic infrastructure can't handle more cars and will make things less safe for pedestrians.
- This site doesn't have great access to transit, especially LRT, that would justify this increase.
- Worried about privacy impacts on nearby houses.

- This type of development drives affordable housing out of the neighbourhood.
- Will shade the north side of University Avenue NW too much, especially in colder months.

## Application Analysis

### The City Plan

The proposed rezoning aligns with two Big City Moves in The City Plan: “A Rebuildable City”, and a “Community of Communities”. Goals associated with a Rebuildable City include adding 50% of net new units through infill city-wide, and welcoming 600,000 additional residents into the redeveloping area. Goals associated with the Community of Communities include: 50% of trips made by transit and active transportation, and the creation of areas that allow people to meet their daily needs within 15-minutes of where they live.

### Scona District Plan

In the Scona District Plan, the site is located within the Urban Mix, with 5 of the 6 current lots in the 109 Street Secondary Corridor. Secondary Corridors are areas along prominent residential and commercial streets that serve as a local destination for surrounding neighbourhoods. They connect to Nodes, feature diverse travel modes and may evolve as more commercial or residential over time.

Policy 2.4.7.2 of the District Policy supports Mid Rise development within Secondary Corridors along Arterial Roadways and Collector Roadways. 109 Street NW is an arterial roadway.

Although one lot is outside of the corridor, expanding the proposed zoning one lot further west will not result in any tangible change in land use impacts. The full 6 lot wide site is approximately 62 metres long which is an appropriate length for a 6 storey building. It should also be noted that this is not an uncommon distance from 109 Street NW for the 109 Street secondary corridor to extend in other places. Directly to the south of this site, the corridor extends this full 6 lot/62 m distance. And while the site is almost entirely within the 109 Street corridor, it is also only a block (just over 100 m) north of the 76 Avenue Secondary Corridor as well making this slight extension more appropriate to accommodate an appropriately sized 6 storey building.

The frontage of the site along 109 Street NW is identified as having a “Commercial Frontage” design influence. Policy 2.4.8.1 of the District Policy encourages commercial land uses at ground-level and street-oriented design to support an active Public Realm along the Commercial Frontage area. The proposed RM h23.0 Zone allows for some commercial opportunities and the applicant is encouraged to pursue these opportunities at the Development Permit stage.



#### *Site analysis context*

There are Area-Specific Policies in the Scona District Plan that apply along 109 Street NW. Though mostly related to streetscape improvements through full road reconstruction in the long term, incremental improvements through private redevelopment are also encouraged. The proposed RM h23.0 Zone allows for future development to meet these policies, which will need to be considered in the building design at the development permit stage. A future building here should have a primary entrance facing 109 Street NW, patios and outdoor seating areas for commercial uses located along 109 Street NW and be designed and constructed to mitigate roadway noise for adjacent residential development.

### **Land Use Compatibility**

The proposed RM h23.0 Zone is considered compatible with zoning along 109 Street NW, which mostly allows for 4 storey residential or mixed use development. The compatibility of this site also benefits from having a very wide road right-of-way north of it where University Avenue NW splits in connecting to 109 Street NW. Properties to the north are between approximately 20 and 33 metres from this site, with shadow impacts at the equinoxes only impacting portions of the front yards of most properties to the north (see Appendix 1 - Sun/Shadow Analysis).

The house to the west of the site, which is zoned Small Scale Residential Zone (RS), will experience the most impacts from this proposed increase in height and expansion of mid-rise zoning further away from 109 Street NW. However, the RM h23.0 Zone has regulations that require the proposed building to respond to this condition and improve this transition. This includes a 3.0 m setback at ground level with an increase to 6.0 m above the 4th storey. In addition, if the facade of the new building is longer than 40 metres in length, these numbers

increase to 6.0 m and 9.0 m, respectively. A comparison of key features of the current and proposed zones are in the table below.

	<b>RS Current</b>	<b>RM h16.0 Current</b>	<b>RM h23.0 Proposed</b>
<b>Typical Uses</b>	Small scale residential	Low rise residential with optional non-residential uses	Mid rise residential with optional non-residential uses
<b>Maximum Height</b>	10.5 m	16.0 m	23.0 m
<b>Maximum Floor Area Ratio</b>	N/A	2.3	3.0
<b>Maximum Site Coverage</b>	45%	N/A	N/A
<b>Number of Dwellings</b>	No minimum 12 maximum (on land currently zoned RS)	8 minimum (on land currently zoned RM h16.0) No maximum	21 minimum No maximum
<b>Minimum North Setback</b> (University Avenue NW)	4.5 m	3.0 m (residential) 1.0 m (non-residential)	3.0 m (residential) 1.0 m (non-residential)

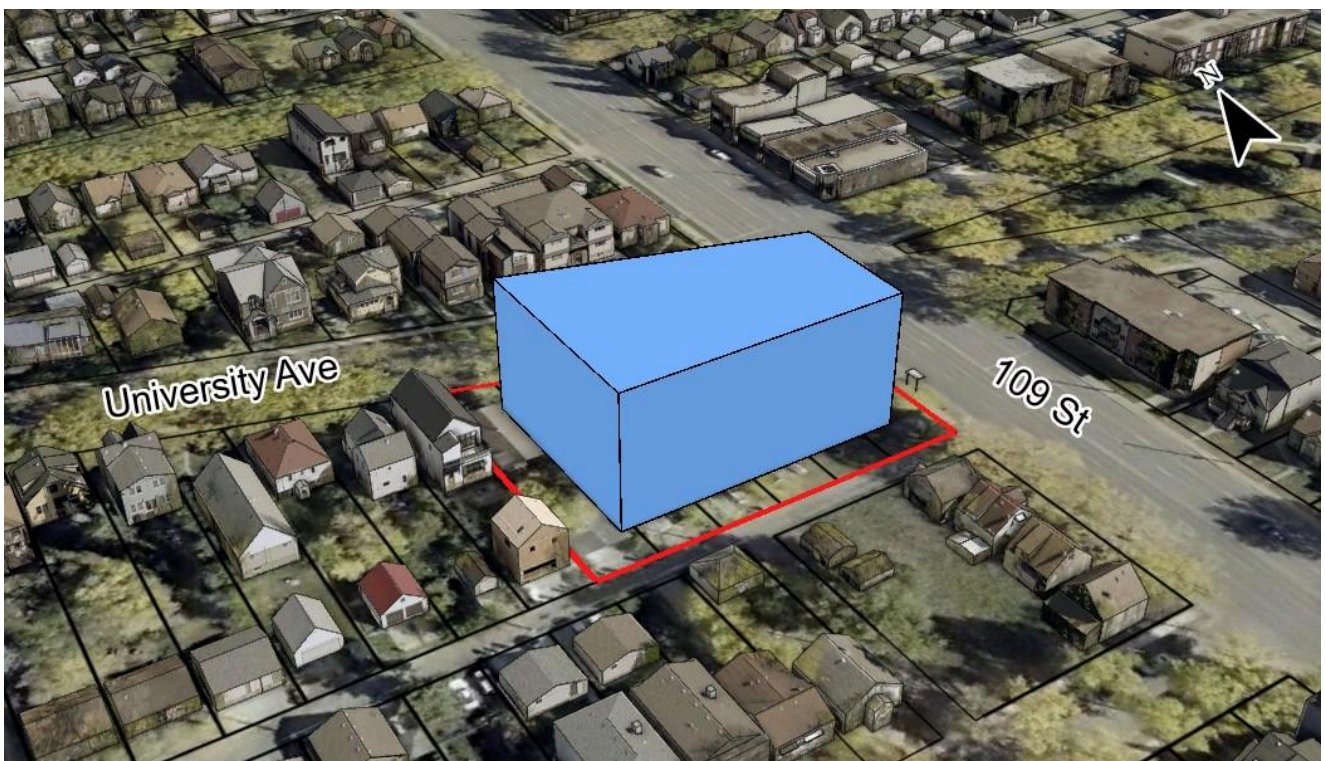
<b>Minimum Interior Side Setback</b>	1.2 m	Wall length 40 m or less: 3.0 m Wall length more than 40 m: 6.0 m	Wall length 40 m or less: <ul style="list-style-type: none"> <li>• 3.0 m</li> <li>• 6.0 m (above 16 m in height)</li> </ul> Wall length more than 40 m: <ul style="list-style-type: none"> <li>• 6.0 m</li> <li>• 9.0 m (above 16 m in height)</li> </ul>
<b>Minimum East Setback</b> (109 Street NW)	N/A	3.0 m (residential) 1.0 m (non-residential)	3.0 m (residential) 1.0 m (non-residential)
<b>Minimum Rear Setback</b> (Alley)	10.0 m	3.0 m	3.0 m

Below are massing models showing the current zones and one version of what could be built in the RM h23.0 Zone. It is important to note that given the site size and dimensions, there are several building designs and site configurations possible under the proposed zone.





*Current Zoning*



*Proposed Zoning*

## **Mobility**

Administration asked the applicant to consider closing a portion of the east-west alley south of the site intersecting with 109 Street NW. This consideration is directed by the Scona District Plan for this area. The applicant declined to include this with their application, but committed to

ensuring the project's design accommodates the possibility of this closure and will not rely on the portion of the alley between the north-south alley and 109 Street NW for access or servicing. Vehicle access is required from the rear alley, and will be reviewed in further detail at the Development Permit stage.

ETS operates frequent and local bus routes on 109 Street NW. A bus stop is located adjacent to the site on 109 Street NW and must remain in its current location with future site redevelopment.

The future mass transit network in the area is anticipated to include Bus Rapid Transit routes operating on 109 Street NW. Design of 109 Street NW is still to be finalized, but infrastructure required on the corridor may include improved sidewalks, upgraded bus stops and dedicated lanes for transit.

## **Risk**

There is an operating high pressure natural gas pipeline near the site under University Avenue NW. At the development permit stage if a sensitive use such as child care services, community services, seniors housing, etc. is proposed, a risk assessment and potential mitigation measures may be required.

## **Utilities**

Development allowed under the proposed zone would be required to include on-site stormwater management techniques utilizing a controlled outflow rate to mitigate its impact on the existing drainage infrastructure. Details of the required stormwater management will be reviewed at the Development Permit stage.

Low Impact Development (LID) is recommended for the development allowed under the proposed zone. Details of any proposed LID would be reviewed at the Development Permit stage.

There is a deficiency in on-street fire protection adjacent to the property in terms of hydrant spacing. The developer will be required to address this deficiency at the Development Permit stage. Edmonton Fire Rescue Services (EFRS) may be able to perform an Infill Fire Protection Assessment (IFPA) at the Development Permit stage to potentially alter or lessen on-street fire protection infrastructure upgrades, assuming certain criteria are met.

The applicant/owner will be responsible for all costs associated with infrastructure changes required by this application.

## **Appendices**

### 1. Sun/Shadow Analysis

Written By: Andrew McLellan

Approved By: Tim Ford

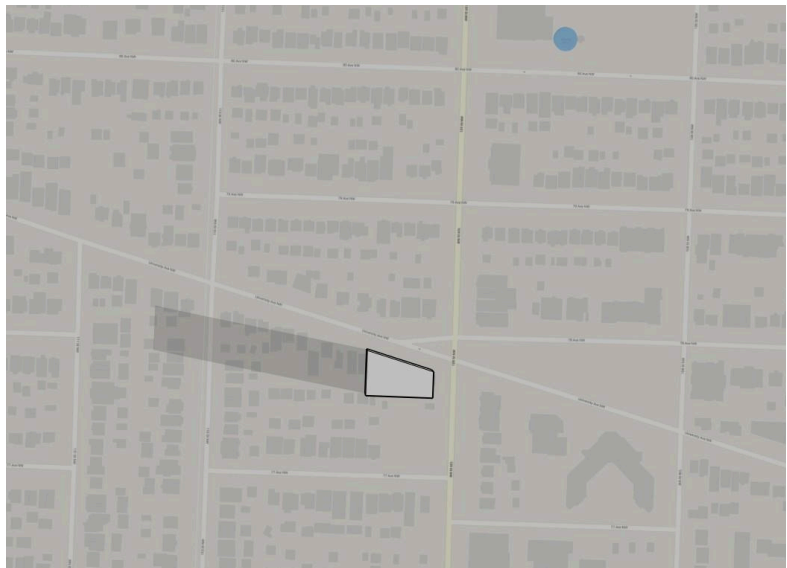
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Section: Planning Coordination

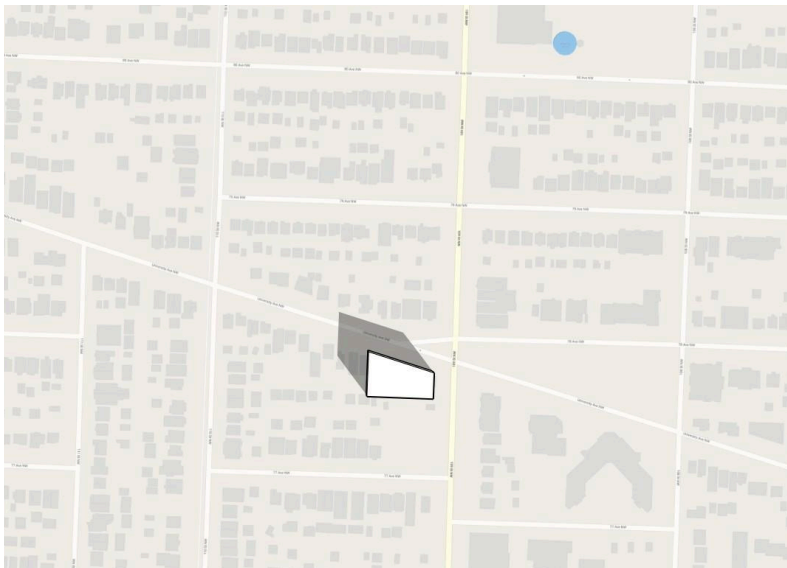


# Sun/Shadow Analysis

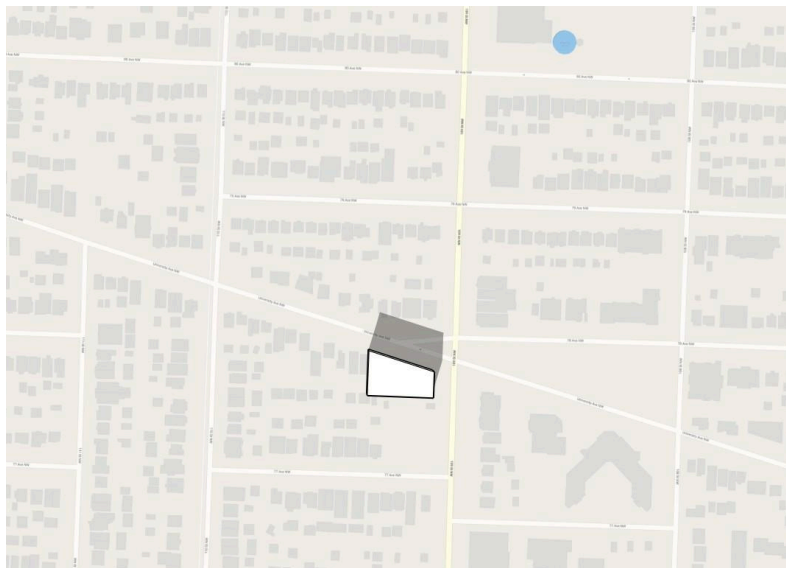
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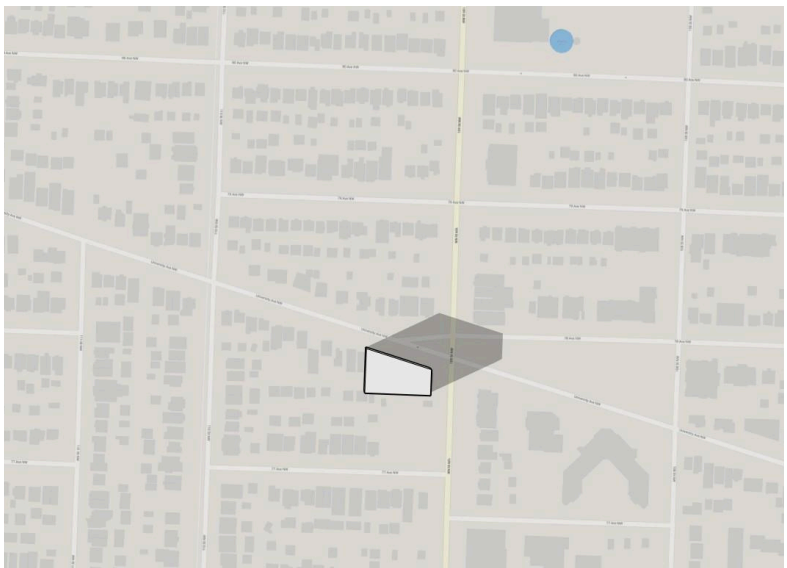
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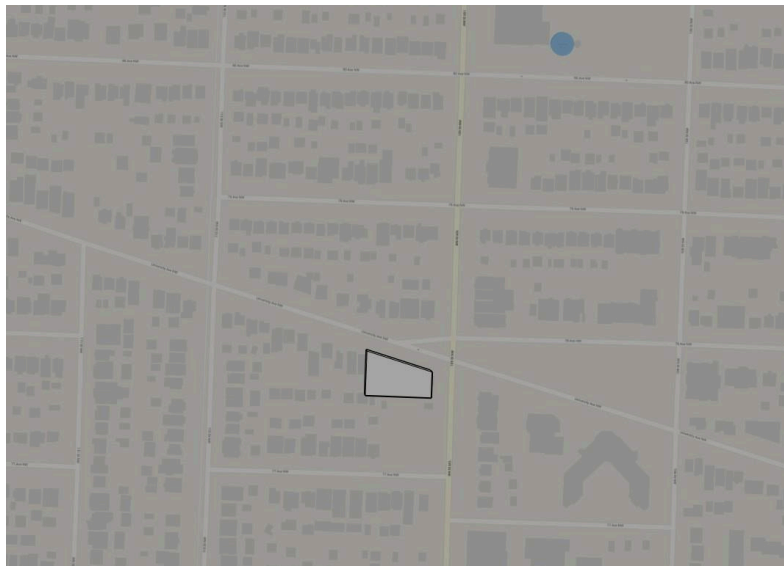
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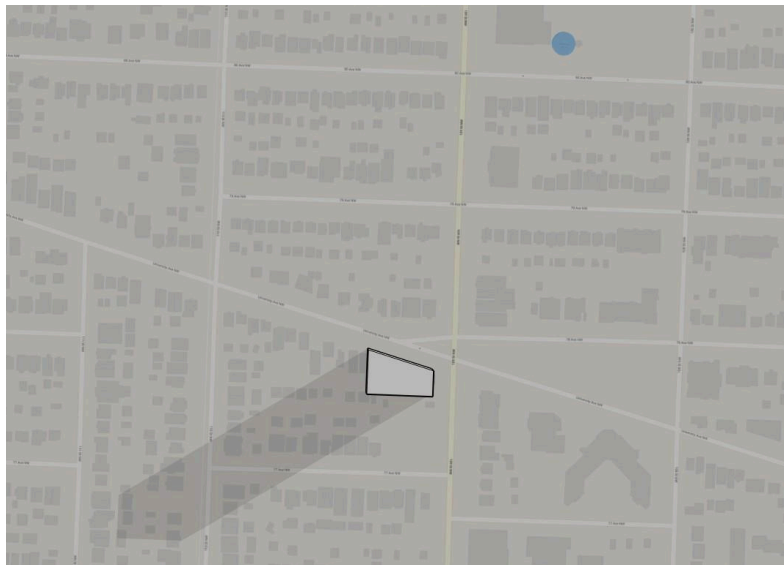


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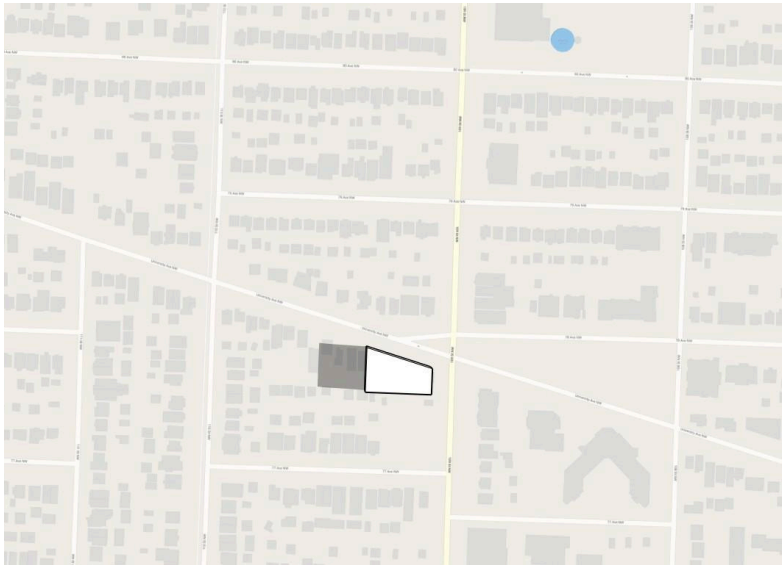


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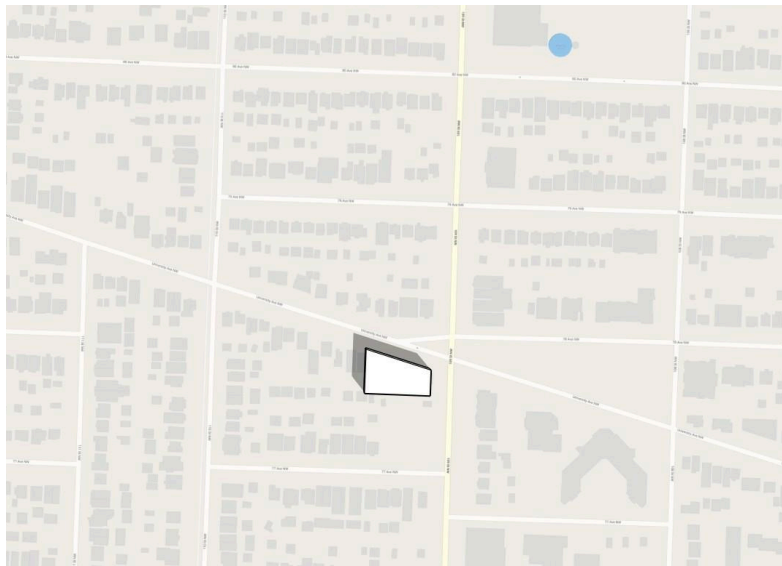
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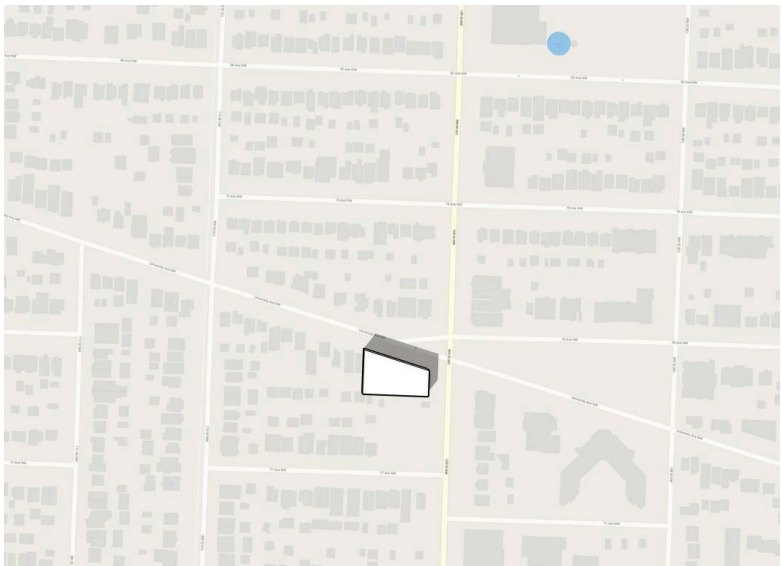
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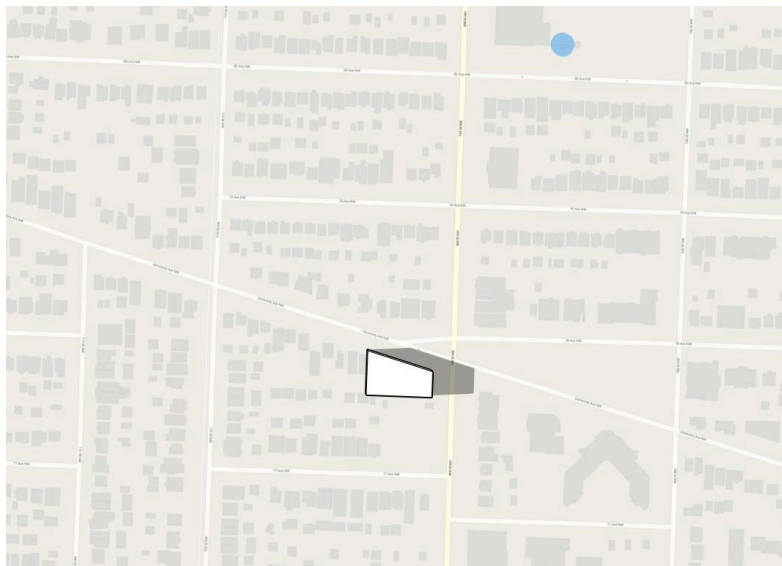
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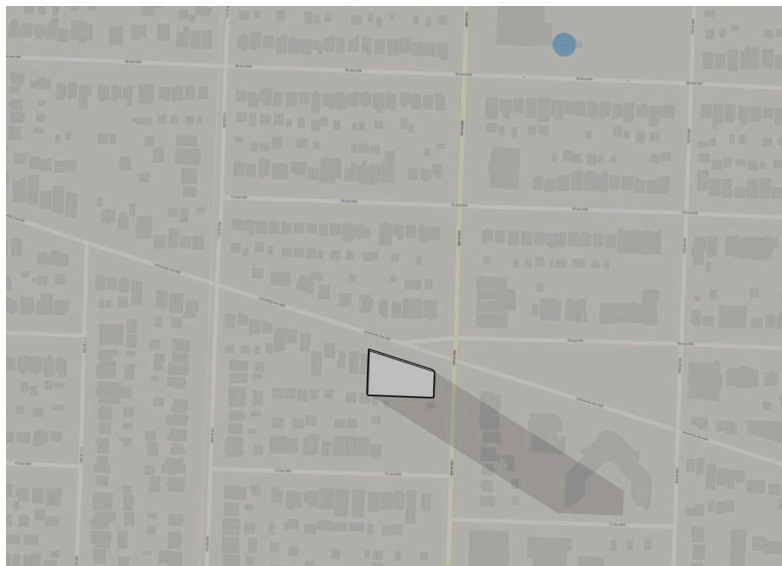
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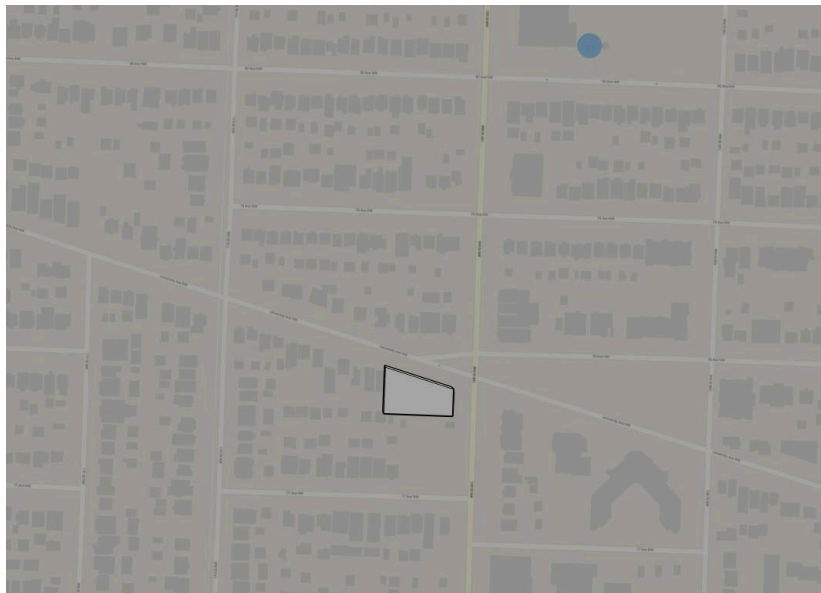


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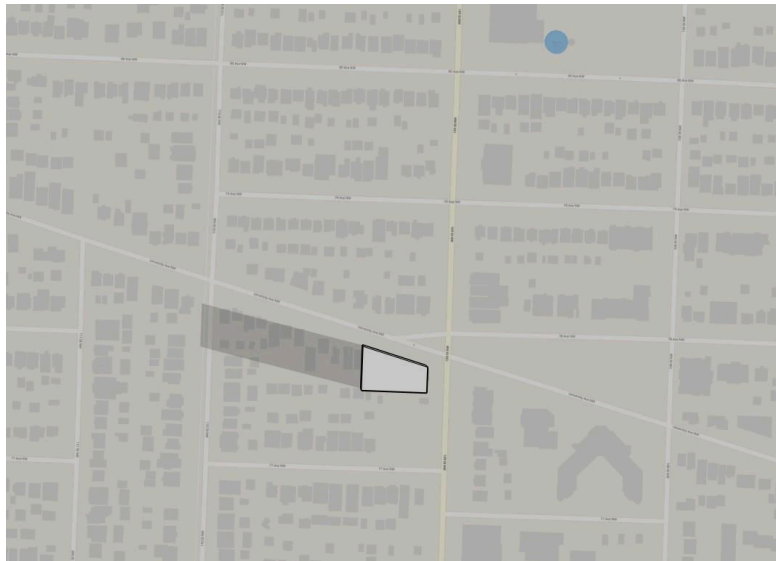
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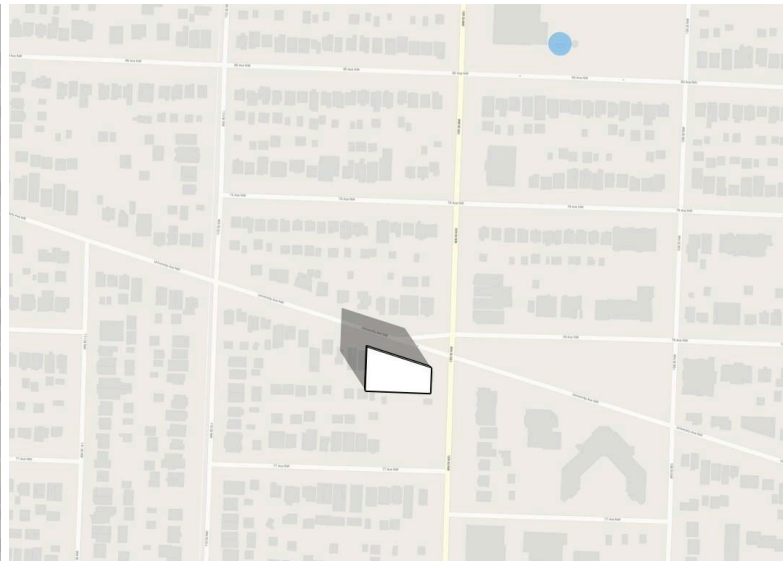


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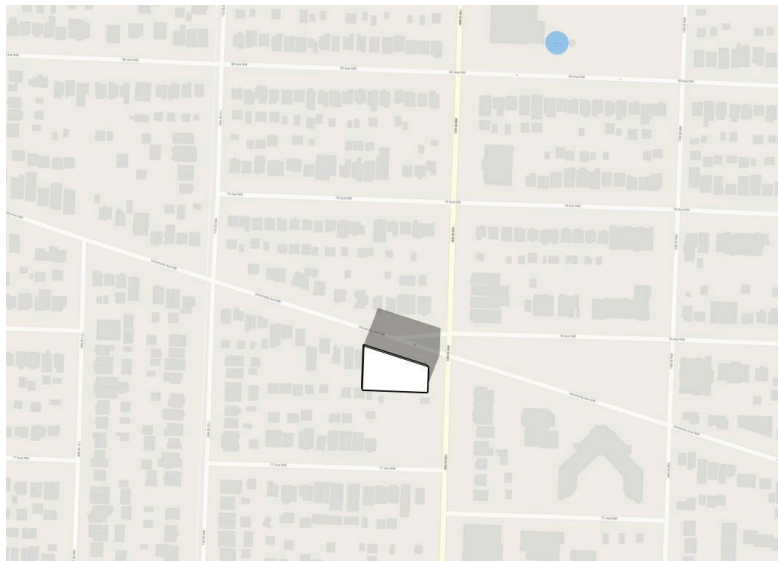
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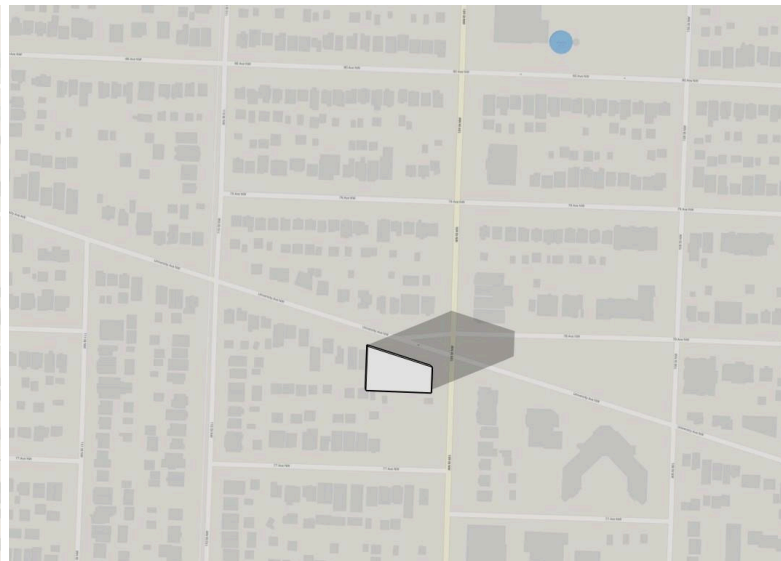
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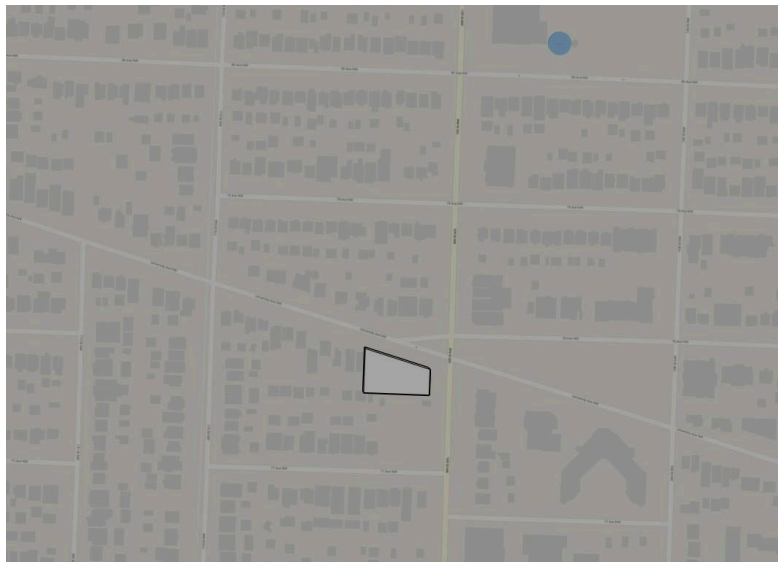
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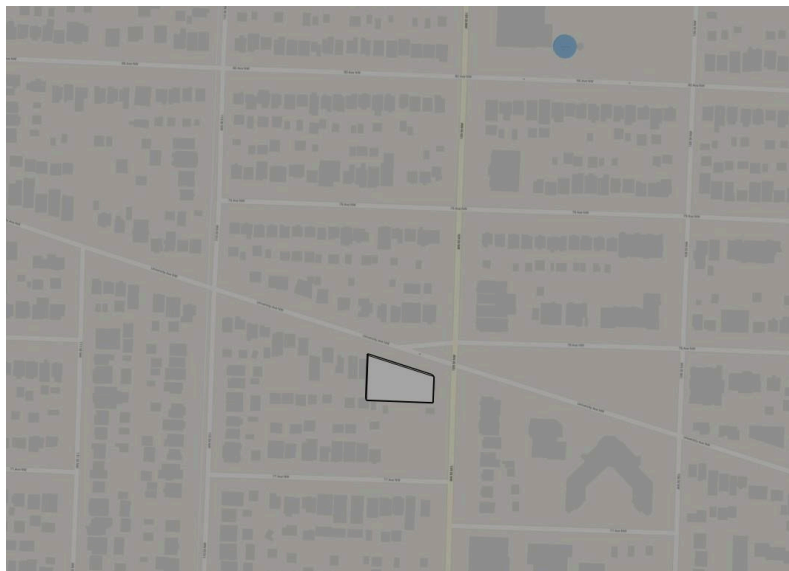


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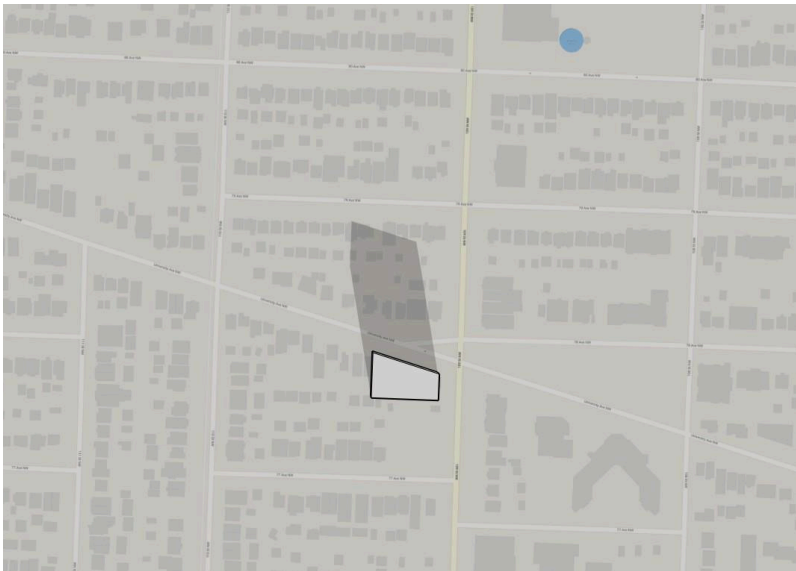


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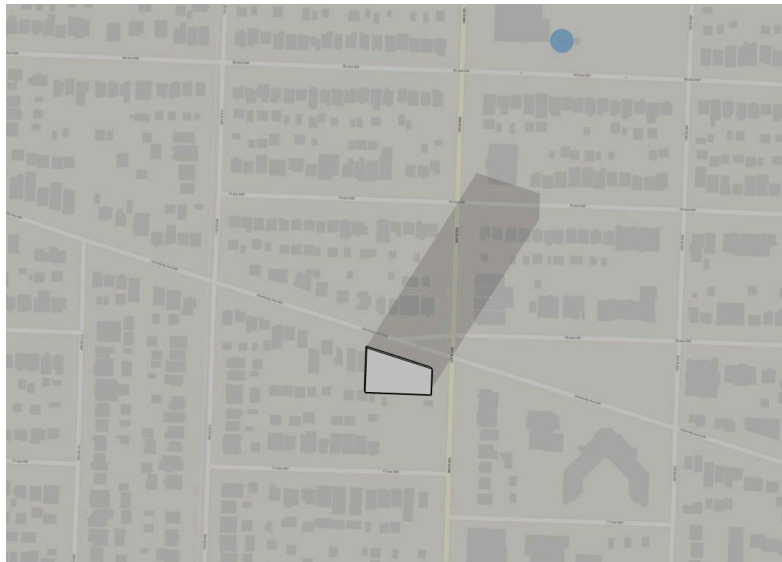
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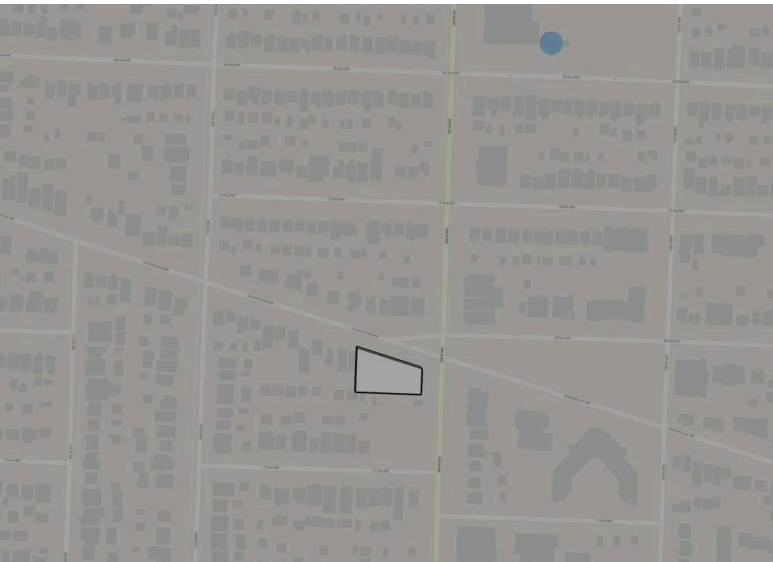
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