

Objective	ASP Policy	Implementation
	(iii) Natural, pre-development hydrological flows to natural areas shall be maintained to ensure the long term protection of the environmental features that are retained through the development process.	(iii) A hydrologic analysis shall be completed at the NSP stage to ensure the long term sustainability of all natural areas retained in the plan area.
3.4.7 (e) To retain key ecological connections within Riverview.	The Riverview ASP shall integrate and link land use components (i.e. North Saskatchewan River Valley and Ravine System, natural areas, SWMFs, parks and greenways) to provide habitat and maintain ecological connectivity.	<b>Figure 6 – Land Use Concept</b> guides the development of subsequent NSPs within Riverview. <b>Figure 7 – Ecological Connectivity</b> identifies potential connections between the natural areas. The ecological network components and connectivity requirements will be confirmed at future NSP stages.
3.4.7 (f) To protect wetlands within the Plan area.	Where possible explore opportunities for the retention of wetlands through a variety of mechanisms including retention, compensation or incorporation with SWMFs.	Wetlands to be retained shall be determined at the NSP stage through additional study.  Opportunities of compensation for wetlands not retained within the Plan area shall be explored at the NSP stage.

### 3.4.8 GREEN DEVELOPMENT & INNOVATION

Low Impact Development (LID) refers to a collection of alternative engineering and landscape architecture strategies to manage and improve the quality of stormwater runoff in an urban environment. Low Impact Development principles replace conventional stormwater management strategies with natural processes. Some LID strategies include constructed wetlands, bioswales, and rain gardens. Some of the benefits of LID include improved groundwater quality and quantity, reduction of river bed erosion and peak flows, reduced costs, and habitat protection.

Native species or (non-invasive) naturalized species should be used in landscaping. The use of plant species which are native or adapted to the local climate generally require less irrigation and maintenance than non-native species, which serves to minimize costs associated with development and maintenance. Appropriate landscaping within open spaces provides opportunities to create wildlife habitat, and strengthens the ecological network within the neighbourhood.

The Leadership in Energy and Environmental Design (LEED) Green Building Rating System™ for Neighbourhood Development (ND) offers an evaluation system that incorporates principles of smart growth, urbanism and green building. Regardless of whether certification under the system is pursued, the required and optional program components offer a comprehensive set of development considerations that can guide neighbourhood design.

A number of alternative energy options will be explored for the Riverview ASP. A geoexchange heating and cooling system, for example, is a renewable alternative to conventional heating systems using gas, oil or electricity. Geoexchange systems take advantage of the heat energy stored in the ground to heat or to cool buildings. Another example would be photovoltaic (PV) systems which are used to convert sunlight into electricity. These systems

produce no pollution or emissions, have low operating costs, and are fairly easy to install. PV systems are increasingly being incorporated into buildings that are also connected to utility systems.

Another option to efficiently cool and heat a series or cluster of buildings would be a district energy system. Benefits of district energy systems can include increased energy efficiency, reduced air pollution and emissions, greater fuel flexibility, facilitation of the use of renewable energy and helping manage the demand for electricity. District energy systems can make use of waste heat generated from other processes, such as the generation of electricity – which is known as combined heat and power (CHP) or co-generation. Similarly, they can be fuelled by industrial by-products like wood chips, biomass or hydrocarbon products or even from municipal refuse. Energy from other renewable sources can be integrated into the systems, including photovoltaic cells, or geexchange systems.

### Technical Summary

No technical study requirements are identified.

Objective	ASP Policy	Implementation
<b>3.4.8 (a)</b> To incorporate sustainable development principles in the planning and design of the ASP and subsequent NSPs.	(i) Where practical, future development in Riverview should explore Low Impact Development (LID) principles such as stormwater infiltration, and reduction and detention of stormwater runoff.	At the NSP stage, appropriate LID and LEED ND principles will be researched and applied where practical.
	(ii) Future NSPs should explore incorporation of appropriate LEED ND principles (or other comparable sustainable development standards).	
<b>3.4.8 (b)</b> To explore alternative energy options in Riverview.	NSPs which feature higher intensity/density land uses (e.g. transit-oriented developments, employment hubs, or education campuses) may consider alternative energy options, such as district heating, combined heat and power systems, geothermal or solar power, where possible.	Evaluate the potential for alternative energy options such as district heating, combined heat and power systems, geothermal or solar power at the NSP level.
<b>3.4.8 (c)</b> To encourage xeriscaping and natural landscaping using native tree and plant species.	(i) Parks, open spaces, and boulevards should incorporate tree and plant species native to central Alberta.	Selection of species shall take place at the detailed design and/or subdivision stage. Landscape design for public land shall be developed in conjunction with relevant City departments, such as Transportation Services and Sustainable Development.
	Where practical, ecotypes from the Riverview area should be used in the landscape design of parks and open spaces.  (ii) Edible landscaping, such as cherry and apples trees and saskatoon bushes, should be incorporated into public parks and boulevards, where appropriate.	

Objective	ASP Policy	Implementation
<p>3.4.8 (d)</p> <p>To utilize Low Impact Development principles for the design of the stormwater management system, including bioswales and naturalized wetlands.</p>	<p>Where possible, neighbourhoods should incorporate Low Impact Development principles that promote stormwater infiltration, filtering, storage, evaporating, in addition to the detention of runoff close to its source.</p>	<p>Land use and servicing plans dealing with stormwater at the NSP level shall employ the City of Edmonton's Low Impact Development Guidelines, or best development practices.</p>
<p>3.4.8 (e)</p> <p>To employ sustainable planning principles in the design of the neighbourhoods.</p>	<p>Each NSP should explore LID, LEED ND principles or other comparable measurable sustainable development standards.</p>	<p>A narrative describing compliance with the chosen standard (or aspects thereof) should be provided to the Sustainable Development Department at the time of NSP submission.</p>
<p>3.4.8 (f)</p> <p>To enhance the quality and quantity of soils in urban areas.</p>	<p>Developers and homebuilders are encouraged to explore innovative topsoil reuse within the ASP</p>	<p>The City and developers should explore the possibility of reuse of topsoil within Riverview ASP or the city.</p>

### 3.4.9 URBAN AGRICULTURE AND FOOD

The Riverview ASP supports *fresh*, the City of Edmonton's Food and Urban Agriculture Strategy, and proposes a number of land uses, policies and approaches to support local food production in the area, to grow the local food market and sustainable food practices.

#### Intensive and Extensive Urban Agriculture

Intensive agriculture refers to high-input, high-output production such as horticulture, greenhouses, specialty crops and fresh vegetable and fruit production. This is a suitable use for urban agricultural land and, at a smaller scale, for urban agricultural land. Extensive agriculture includes such activities as pasture, grazing, and to some extent large scale field crops that are typically grown in dry land situations (without irrigation). Extensive agriculture is not considered ideal for urban or peri-urban agriculture.

#### Local Food Economy

While Edmonton's climate cannot support year-round outdoor growing, developing innovative urban food production, processing and storage strategies can be appropriate for the climate and context. Supporting local food production and processing in the Capital Region helps contribute to the development of the local economy, and can encourage a healthy and sustainable food system.

#### Value-Added Food Production

Value added agriculture refers to the processes that increase the value of raw agricultural products such as pickling, curing, canning and/or preparing industrial or consumer ready food products. Value-added industries can occur on different scales, from the home-based business selling jams and pies to national brand consumer foods. While much of Edmonton's food processing occurs in industrial areas, there are ways to promote the use and production of local foods in Riverview.

Community kitchens, for instance, are an important component of building strong, healthy, and sustainable communities. As economic development incubators, community kitchens provide food entrepreneurs with access to

the tools and resources to add value to local produce and distribute their products in the community, city, and region. These spaces have been key starting points for many small-scale food entrepreneurs to generate revenue and transition to larger-scale production. Community kitchens provide space for residents preparing large quantities of food for home catering businesses and religious services. Community kitchens serve as a space to promote literacy in healthy eating and food preparation for community courses, and should be included in community centres or recreation facilities to benefit from other education and recreation programming.

### **Home Gardens**

Home gardening is both an exceedingly popular pastime and a means of producing vegetables and fruit throughout the summer months. One of the simplest and most apparent examples of small scale local food production is the traditional back yard garden. The amount of land that is available for gardening in low density residential areas is considerable, and even apartment housing on private amenity spaces such as balconies or shared open space could be used for garden plots.

### **Community gardens**

Community gardens are features that are found in many neighbourhoods throughout the city. Community gardens are generally divided into individual plots which are made available to the public or members of the community, often for a nominal fee or for no cost. These spaces serve multiple purposes – from social gathering spaces, to vegetable gardens, to ornamental flower gardens – and are often popular in locations with higher density housing where private open space is limited. As Community Services recognizes gardening as a legitimate recreational pursuit, some Municipal Reserve land within neighbourhood parks may be dedicated for community gardens throughout the Riverview area. Organizations such as Community Leagues, non-profit societies, residents associations or faith groups are often willing to administer community gardens, likely with minimal support from Community Services.

### **Edible Landscaping**

In addition to private gardens, public spaces can provide attractive and productive land. Public lands such as parks and open spaces, or even road and utility rights of way, can be planted with a broad variety of edible species. These might include fruit trees, berries like saskatoons, high-bush cranberries, raspberries, nuts, or other plants like rhubarb. Expanding the variety of plants in public spaces beyond ornamental species is a fairly easy way to increase the range of potential uses, and to provide a source of local food.

### **Community-Supported Agriculture**

Community-Supported Agriculture (CSA) connects agriculture producers with the consumers in a food distribution network. A typical CSA arrangement is a shared-risk agreement that sees a farmer offer memberships to consumers who then receive a regular delivery of seasonal produce directly from the farmer. This arrangement removes the middle-person from the supply chain and connects the consumer with the farmer. In turn, farmers benefit from the regular and increased cash flow, and consumers benefit from access to health, locally growth food.

### **Urban Agriculture and Beekeeping**

Ways of promoting healthy eating and sustainable food practices are re-emerging in urban areas. Urban agriculture, for example community gardens, community orchards, food forests and berry patches, allow food to be grown locally and contribute to sense of community. Several cities now allow the keeping of a limited number of hens for the purposes of laying eggs, as well as the keeping of bees. Beekeeping, whether located on farms, backyards or urban rooftops, can contribute to a healthy urban landscape by contributing to the pollination of plant species. Should the City of Edmonton pursue changes to current bylaws and regulations to allow beekeeping and backyard hens, the Riverview ASP would be supportive of those uses, as well as opportunities for community orchards, food forests, berry



patches, hydroponics, SPIN farming, aquaponics, container gardens, and vertical growing systems, among other methods for urban agriculture.

### Land use compatibility

As more detailed planning occurs, methods to mitigate potential nuisance factors (e.g. noise and dust from farming, traffic and encroachment from residential development) created by intensive agricultural operations will be investigated. Possible solutions include setbacks, shelterbelts, multi-use trails, park space, or roadways. In addition to buffering, the integration of compatible land uses should be considered.

### Technical Summary

No technical studies were identified.

Objective	ASP Policy	Implementation
3.4.9 (a) To allow for retention of land for urban agriculture.	<p>i) Land may be retained for urban agricultural use.</p> <p>(ii) Where lands are designated urban agriculture through future amendment of this ASP, farming activities shall be limited to intensive agriculture operations, and will not include intensive livestock operations, extensive agriculture operations, or other high-impact operations.</p>	<p>Agricultural Land Reserve (ALR) designation may be applied for by landowners of privately owned farmland who self- declare their intent to retain urban agricultural use within the ASP area.</p> <p>The ALR designation in the ASP may be applied for, at the discretion of the landowner.</p>
3.4.9 (b) To buffer urban agriculture uses and non-agriculture uses.	<p>(i) Provide appropriate buffers or compatible land uses adjacent to retained urban agricultural parcels.</p> <p>(ii) Where agricultural operations predate new developments, the responsibility for buffering shall fall to new land uses.</p>	<p>If applicable, NSPs shall provide guidelines for appropriate buffers, setbacks or compatible uses adjacent to retained urban agricultural parcels.</p>
3.4.9 (c) To promote urban agriculture in Riverview.	<p>(i) Opportunities for community garden plots are encouraged on publicly-owned parcels.</p>	<p>(i) The location, number and size of community gardens should be further developed at the NSP level. The land requirements for public community gardens shall be reviewed and determined at the NSP stage.</p>
	<p>(ii) Community gardening is recreational, a public benefit, builds community and should be included in programming for community parks in balance with other park programming needs.</p>	<p>(ii) Community leagues and other voluntary organizations, such as Sustainable Food Edmonton, should be encouraged to be the stewards of community gardens and develop best practices for urban agriculture.</p>
	<p>(iii) Explore opportunities to utilize harvested rainwater for irrigation of parks and community gardens.</p>	<p>(iii) Specific implementation strategies pertaining to sustainable practices shall be determined at the NSP stage, and in conjunction with the appropriate City Departments.</p>

Objective	ASP Policy	Implementation
	(iv) Landscaping of public parks and open spaces should consider planting of edible fruit and vegetable plants where appropriate.	(iv) Edmonton's edible landscaping guidelines (to be developed) should be adhered to at the neighbourhood planning level and during the design of parks and open spaces.
	(v) Opportunities for urban agriculture, for example community gardens, community orchards, food forests, berry patches and beekeeping in the Riverview area may be explored in NSPs and through City bylaws and policies.	(v) Opportunities for urban agriculture such as keeping bees, raising backyard laying hens, community orchards, food forests, berry patches, hydroponics, SPIN farming, aquaponics, container gardens, and vertical growing systems, among other methods for urban agriculture shall be explored and implemented in accordance with <i>fresh</i> .
	(vi) Investigate urban agriculture opportunities on utility rights-of-way.	(vi) Developers and/or City Administration shall investigate the feasibility of using utility corridors (e.g. electrical transmission lines or transportation corridors) for urban agriculture.
	(vii) explore opportunities for additional urban agricultural land.	(vii) The City and partners shall explore opportunities to identify and secure new urban agricultural lands. Examples include using an Agricultural Land Reserve (ALR), Transfer of Development Credit (TDC), Land Trust, Conservation Covenant, Land Swap, Community Investment, Cluster Development, zoning and land use designations, and land/financial contributions by developers
3.4.9 (d) To support the development of local food infrastructure.	(i) The Mixed Uses / Main Street Retail area should include public space for food related infrastructure which could include a community farmers' market, pocket park, street vendor sites, or sidewalk patios.	(i) Planning at the NSP level shall provide guidance for the development of the Mixed Uses / Main Street - town centre area, including the public space.
	(ii) Support the development of pocket markets, mobile markets, and mobile food vendors.	(ii) Public and quasi-public spaces (plazas, recreation centres, shopping centre parking lots, or streets) can accommodate small scale food operations. Policies supporting these and other food system assets should be provided at

Objective	ASP Policy	Implementation
		the NSP stage.
	(iii) Support the development of food hubs in the Riverview ASP, as directed by <i>fresh</i> .	(iii) Food hubs vary in type, scale, and model, but all opportunities and partnerships to establish food hubs should be encouraged. If applicable, additional policies supporting food hub uses should be provided at the NSP stage.
3.4.9 (e) To establish community supported peri-urban agriculture opportunities in Riverview.	(i) The Mixed Uses / Main Street Retail - town centre should include a public space for a community farmers market.  (ii) Community leagues should be encouraged to be the stewards of community gardens and develop best practices for urban agriculture.	Strategies pertaining to peri-urban agricultural activities will be developed in accordance with City-wide Food and Agriculture Strategy of the City of Edmonton provided at the NSP stage.

### 3.4.10 ENVIRONMENT

Lands within the Riverview ASP Plan area will be suitable for urban development and their environmental status will be confirmed prior to rezoning. Those lands identified as contaminated must undergo remediation according to Federal, Provincial, and Municipal standards.

Policies relating to existing and abandoned oil and gas uses will ensure conscientious residential development around oil and gas well sites and pipelines at all stages of the plan implementation and construction process while minimizing potential disturbances to the area's future residents. Urban development in the vicinity of all resource well sites will be planned in accordance with the City policy document entitled "Policy Guidelines for the Integration of Resource Operations and Urban Development" (1985) and Policy C515 "Oil and Gas Facilities" (2007) and other relevant City procedures. Development of lands involving abandoned wells shall comply with ERCB guidelines for development around abandoned wells. An assessment of risk and nuisance will be conducted on operating or suspended oil and gas wells, as directed by existing or future City policy for the integration of oil and gas facilities prior to any rezoning of the parcel where the facility is located.

#### Technical Summary

An Environmental Overview report has been completed for the Riverview area to confirm that the Plan area will be suitable for development. An environmental site assessment will be completed at the NSP stage.

Objective	ASP Policy	Implementation
3.4.10 (a) To ensure that the environmental status of the lands within Riverview is suitable for development.	(i) The likelihood, types, and location of any environmental concerns present on the lands shall be determined prior to rezoning.	(i) Phase I Environmental Site Assessments (ESA) shall be required for participating landowners at the NSP stage.

		<p>ESAs shall be submitted and any follow-up will receive sign-off by city Administration prior to the rezoning stage of development.</p> <p>Phase I ESA reports older than 1 year from the date of rezoning application shall be updated, and any Phase I ESA report older than 5 years from the date of rezoning application shall be redone.</p>
	<p>(ii) Where necessary, contaminated material shall be removed and disposed of in an environmentally sensitive manner, in accordance with Federal, Provincial, and Municipal regulations.</p>	<p>(ii) Site remediation, where necessary, shall be conducted prior to rezoning. An environmental site assessment report verifying the remediation shall be submitted for approval by the City administration prior to the rezoning of the subject lands.</p>
<p>3.4.10 (b) To minimize oil and gas well sites' potential environmental hazards and disruption of future development.</p>	<p>The Riverview ASP and future NSPs shall be designed to comply with relevant requirements of the ERCB regulations, the Urban Parks Management Plan and other relevant City of Edmonton policies.</p>	<p>Setback requirements shall be confirmed at time of subdivision approval. Subdivision planning must adhere to all relevant ERCB regulations, City of Edmonton Policy C515, and the Urban Parks Management Plan.</p> <p>An assessment of risk and nuisance may be required at the NSP or rezoning stages.</p>

**3.4.11 HISTORICAL RESOURCES**

Riverview likely contains numerous historical artifacts and archeological resources. Identification and protection of historical resources is important for preserving Alberta's culture and understanding the history of the land and the people who called it home.

Development within the Riverview ASP shall have regard for the preservation of historical, archaeological and paleontological resources. Pursuant to Section 31 of the Historical Resources Act, development proponents and/or their representatives are required to report the discovery of any archaeological, historic period or paleontological resources, which may be encountered during construction.

**Technical Summary**

A Statement of Justification for Historical Resources Act has been submitted to Alberta Culture and Community Spirit.

Objective	ASP Policy	Implementation
<p>3.4.11 (a) To ensure historical, archeological, and paleontological resources are</p>	<p>(i) Proponents of the Riverview ASP shall submit a Statement of Justification for Historical Resources</p>	<p>(i) A Statement of Justification has been completed for eight parcels within the Plan area and approved</p>

Objective	ASP Policy	Implementation
identified and, where applicable, conserved.	Act Requirements and, if necessary, a Historical Resources Impact Assessment (HRIA).	<p>by Alberta Culture and Community Spirit.</p> <p>Where required, Historical Resources Impact Assessments (HRIA) shall be submitted prior to rezoning approval.</p> <p>Those lands which have not received clearance on Historical studies shall be required to submit and receive sign off prior to rezoning.</p>
	(ii) All historical, archaeological, and palaeontological discoveries made during construction shall be reported.	(ii) Section 31 of the Historical Resources Act requires all historical, archaeological, and palaeontological discoveries made during the course of an excavation to be reported to Alberta Culture and Community Spirit. Those discoveries that are notable or deemed significant enough, should be incorporated or interpreted within the development scheme/proposal in a manner that ensures its historical significance is appropriately interpreted and/or displayed. Proposals should be submitted to the City's heritage planners in consultation with Alberta Culture and Community Spirit.

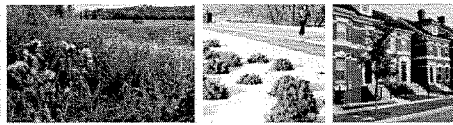
### 3.4.12 TRANSPORTATION

The transportation goal of the Riverview ASP is to establish a neighbourhood that provides a well-connected transportation system within the Plan area, and maximizes access opportunities for multiple modes of transport for residents. This goal is conceptually illustrated in Figure 9 – Roadway Network and Figure 10 – Active Transportation Network, which identify the roadway network and alternative system that accommodates the movement of transit, bicycles and pedestrians within the neighbourhood and connects to the River Valley and adjacent communities.

#### Roadway Network

The transportation network has been designed to meet both the internal and external traffic flow requirements generated by the neighbourhood in accordance with City of Edmonton's guidelines and standards. A hierarchy of arterial, collector and local roadways is intended to facilitate the efficient movement of vehicular traffic (see Figure 9 – Roadway Network).

Arterial roadways generally form boundaries of future NSPs. These roadways, which accommodate the ASP's major internal/external traffic flows, include 23 Avenue, 199 Street, and Winterburn Road (215 Street).



The interchange on Anthony Henday Drive at 23 Avenue NW will provide access opportunities from the ASP to major external destinations. In addition, Winterburn Road (215 Street) and 199 Street will accommodate additional north-south traffic movements to/from the ASP.

Collector roadways, which provide internal/external accesses, are spaced at appropriate intervals to facilitate traffic progression (if traffic signals are required) and to ensure that sufficient distance is available to allow for right and left turn-bay development. The collector roadway network provides efficient and convenient access to residential areas, reinforces a local 'sense of place' among residential sub-areas, and establishes a pedestrian-oriented streetscape through provision of separate sidewalks.

### **Arterial Roadway Assessment**

Lands within the Riverview ASP shall be subject to an Arterial Road Assessment (ARA) to cost-share the arterial roadway facilities needed to service the area.

In general terms, the ARA outlines the developers' responsibility for roadway construction within a catchment area and is based on the costs of constructing arterial roads required for access to a catchment area. The configuration (number of lanes) and other specific criteria (such as channelization) required for access to the Riverview ASP are defined in detail by the associated Transportation Impact Assessment.

### **Parking**

Parking for vehicles will generally be provided off-street in conjunction with development. The land use concept identifies the District Park adjacent to the North Saskatchewan River Valley to promote access to the River Valley and accommodate parking for visitors. The parking supply on the district park site should be rationalized through the completion of a shared use parking assessment and should consider parking demands associated with on-site uses as well as parking demands associated with visitors to the River Valley.

Future NSPs may identify additional parks along the Top-of-Bank that may function as staging areas for the River Valley by incorporating visitor parking.

### **Public Transit**

Public transit services will be extended into the ASP area in accordance with City of Edmonton Transit System Guidelines and demands. The neighbourhood has been designed such that a majority of the residential areas will be within 400 m walking distance from a transit route. Walkways will be added in the plan area in an effort to reduce walking distances to transit.

Transit service will be accommodated within the neighbourhood as demand warrants. Developer funded transit service will be explored within a Neighbourhood Structure Plan and if warranted, initiated at the time of development. Internal collector roadways will be developed to a suitable standard to accommodate transit service and provide readily accessible service to all areas of the neighbourhood.

### **Transit Oriented Development**

Transit Oriented Development (TOD) concentrates housing, shopping and employment along a network of walkable and bikeable streets within a five-minute walk in any direction of a transit station (or 400 metres). The City TOD Guidelines and policy for development adjacent to the proposed Transit Centre shall be followed and rationally integrated at the NSP stage.

### **Pedestrian and Bicycle Network**

The Riverview area will benefit from an increased level of pedestrian connectivity. All local and collector roadways within the ASP shall be developed with sidewalks, providing a general level of access throughout the neighbourhood. Bicycle traffic within the ASP is designed to follow collector and local roadways. Bicycle routes may be integrated with shared use path corridors and walkways connecting internal and adjacent residential areas and amenities. Routes will be clearly marked using appropriate signage and markings in order to minimize potential conflicts between vehicles, cyclists, and pedestrians in the neighbourhood.

The provision of pedestrian access to the Transportation Utility Corridor (TUC) and a shared use path within the TUC to connect to the existing shared-use bridge at Anthony Henday Drive to the Riverview Top-of-Bank shared use path will be explored by the City of Edmonton and/or developer(s) and the Province at the NSP or subdivision stage. If permission for a path and walkways is granted by the Province, a shared use path and the associated walkways will be built by the developer at their expense with adjacent subdivisions.

### **Greenways**

Subsequent NSPs will provide a number of greenways linking key locations within the Riverview area, which will provide pedestrian and active transportation routes that are separate from vehicle routes. Greenways function as shared use path (SUP) corridors and are intended to be approximately 10 m wide with a 3 m wide paved path, and will also feature grassed and/or naturalized planting, park furniture (e.g. benches, waste receptacles), trees and shrubs, as well as interpretive and directional signage.

### **Top-of-Bank Shared Use Path / Roadway**

A Top-of-Bank (TOB) shared use path shall be provided along the entire length of the Urban Development Line, between the Top-of-Bank line and the Urban Development Line. It will feature a 3m wide paved shared use path, unless topographic limitations necessitate an alternative design. There will be multiple access points from residential areas by way of walkways and from several points along the roadway system. The shared use path will accommodate pedestrian and bicycle access along portions of the North Saskatchewan River Valley and Ravine System.

The top of bank roadway will be provided along a minimum of 30% of the length of the Urban Development Line.

### **Transportation Mode Shift**

As an alternative to expanding roadway cross-sections to accommodate passenger vehicle traffic, opportunities to promote a mode shift to transit and active modes will be explored. These opportunities may include the recommended use of premium transit options, the implementation of Transportation Demand Management (TDM) programs, and the implementation of active mode strategies. These opportunities will be reviewed in further detail at the NSP stage. There is a significant opportunity to reduce passenger vehicle traffic within and external to the plan area with the successful implementation of transit, TDM, and active mode strategies.

### **Truck Routes**

In the vicinity of the plan area, 215 Street north of 23 Avenue is designated as a 24 hour truck route and Anthony Henday Drive is designated as a dangerous goods truck route. Eventually, 23 Avenue through the plan area may be designated as a 24 hour truck route.

### **Noise Attenuation**

Noise attenuation will be provided for all residential lots adjacent to arterial roadways and Anthony Henday Drive in accordance with the Urban Traffic Noise Policy.



### Technical Summary

A Transportation Impact Assessment (TIA) has been submitted under separate cover for review and approval by Transportation Services.

Objective	ASP Policy	Implementation
<b>3.4.12 (a)</b> To create a creative, multi-modal transportation network with a high degree of connectivity	(i) The ASP shall provide a high degree of connectivity and access for active modes of transportation.	(i) <b>Figure 9 – Roadway Network</b> and <b>Figure 10 – Active Transportation Network</b> illustrate the multiple modes of transportation provided within the ASP.  NSPs shall provide a network of collector roadways, shared use paths and walkways , and explore opportunities for a grid network.
	(ii) Provide community bus service to facilitate movement within Riverview.	(ii) Provision of community bus service for Riverview shall be explored at the NSP stage in discussions with the City of Edmonton.
	(iii) Where feasible, provide roundabouts at key roadway intersections within Riverview.	(iii) NSPs will investigate the potential to provide roundabouts at key arterial and/or collector intersections.
<b>3.4.12 (b)</b> To create a pedestrian- and cyclist-friendly environment.	(i) Roadways should accommodate pedestrians and cyclists. Trails, paths and walkways should supplement, not replace, shared use roadway facilities.	(i) NSPs shall provide a network of collector roadways, shared use paths and walkways.
	(ii) NSP transportation concepts shall include a system of greenways/shared use paths, TOB shared use path, and walkways to facilitate pedestrian movement across the neighbourhoods and to the River Valley.	(ii) & (iii) Neighbourhood design at the NSP level shall have regard for pedestrian movement and connections.
	(iii) The stormwater management facilities shall accommodate paved shared use paths.	
	(iv) Front drive access for ground-oriented residential units along arterial roadways shall not be permitted.  Front drive access for ground-oriented residential units along collector roadways within 400 m of the Transit Centre should be minimized.	(iv) Conformance with this policy will be reviewed at the subdivision stage.
<b>3.4.12 (c)</b> To ensure roadways do not	Roadways passing through key ecological	Transportation Services, in

Objective	ASP Policy	Implementation
interrupt significant wildlife corridors.	<p>connections shall be designed in accordance with the Wildlife Passage Engineering Design Guidelines to mitigate impacts.</p> <p>The potential wildlife passages are identified in the <b>on Figure 7 – Ecological Connectivity</b> and are subject to more detailed study at the NSP stage.</p>	<p>conjunction with the Office of Biodiversity, shall review the design of wildlife passages at the concept plan stage for arterials, and at subdivision for other roadways.</p>
<p>3.4.12 (d) To ensure that cost of arterial roadways is shared throughout the Riverview catchment area.</p>	<p>Lands within the Riverview ASP shall be subject to an Arterial Road Assessment (ARA) to cost-share the roadway facilities needed to service the area.</p>	<p>City Administration will prepare an Arterial Road Assessment Bylaw for the Riverview ASP area.</p>
<p>3.4.12 (e) To ensure integrated access is provided to the North Saskatchewan River Valley and Ravine System.</p>	<p>Public access to the North Saskatchewan River Valley and Ravine System shall be provided through a combination of Top-of-Bank roadway and viewpoint parks along a minimum of 30% of the length of the Urban Development Line. A continuous Top-of-Bank walkway shall be provided along the length of the Urban Development Line, within the Public Upland Area. In area of existing residential development where subdivision has already occurred, a Top-of-Bank shared use path cannot be provided. In such cases, an alternate connectivity internal to the neighbourhoods shall be provided.</p> <p>The design of the neighbourhood shall comply with City Policy C542 Development Setbacks from River Valley/Ravine Crests.</p>	<p>A combination of TOB roadway and park will be provided for a minimum of 30% of the entire length of the Urban Development Line and shall be implemented through the subdivision process, with the Urban Development Line being identified on participating lands at the NSP stage.</p> <p>Where Existing Country Residential prohibits the continuation of the TOB walkway/roadway adjacent to the Urban Development Line alternate connections shall be determined at the NSP stage to ensure a connection between the TOB walkway/roadway on either side of the existing development.</p> <p>Potential access points to the River Valley are illustrated in <b>Figure 9 – Roadway Network</b>.</p> <p>Geotechnical/slope stability assessment and City Policy C542 shall determine the final alignment of the TOB roadway, TOB walkway and viewpoint parks. TOB roadway, TOB walkway and parks shall be dedicated to the City of Edmonton at the time of subdivision. Compensation for the development of TOB roadway shall adhere to City Policy C542.</p>

Objective	ASP Policy	Implementation
3.4.12 (f) To encourage innovation in transportation systems design.	New and innovative roadway cross-sections should be explored to improve cost-effectiveness and to ensure that form follows intended function.	Where required, alternate roadway cross sections will be prepared for review and approval by Transportation Services, prior to subdivision approval.
3.4.12 (g) To plan for Transit Oriented Development around the Transit Centre.	Development surrounding the Transit Centre shall be developed in accordance with the City's Transit Oriented Development Guidelines.	NSPs shall plan for Transit Oriented Development in areas adjacent to the Transit Centre.
3.4.12 (h) To facilitate snow removal and storage in Riverview.	(i) Incorporate boulevards into the design of roadways.	(i) Roadways designated for boulevards with separate sidewalks will be identified within NSPs.
	(ii) Encourage practical uses for cleared snow, such as building wind breaks in parks.	(ii) Snow removal strategies will be reviewed with applicable City Departments at the NSP stage.
3.4.12 (i) To encourage Transit and Active Modes as alternative to single occupant vehicle travel.	Developers shall work with City Administration to encourage alternative modes of travel by exploring incentive programs to increase transit ridership, transportation demand management programs, as well as opportunities for premium transit, in addition to ensuring that the appropriate infrastructure is provided within the roadway network to allow for active modes of transportation.	Developers shall work with City Administration at the NSP stage to develop a transportation strategy including infrastructure and programming needs, timing, cost sharing or funding.
3.4.12 (j) To emphasize pedestrian-friendly environments in key locations.	Streets within the Commercial / Retail with Main Street area shall be designed as pedestrian-oriented streets and is encouraged to incorporate elements/techniques such as human-scale design; traffic calming measures; weather protection, wide sidewalks, pedestrian oriented furnishings, etc.	Transportation Services, in conjunction with the Sustainable Development Department, shall review proposed roadway designs.

### 3.4.13 INFRASTRUCTURE, SERVICING, AND STAGING

The ASP will be designed in accordance with City of Edmonton servicing standards. Development staging and extension of infrastructure will be contiguous, efficient, and economical while mitigating potential environmental and ecological impacts. Opportunities for research and innovation should be supported in order to discover efficient, low cost or low environmental impact servicing options.

#### Sanitary Drainage

Sanitary servicing for the Riverview area consists of a series of trunk sewers that flow by gravity from the south to the north within the proposed road rights-of-way to the intersection of 23 Avenue NW and 184 Street NW. From this

point the sanitary flows will be directed to the SESS system through a large diameter gravity trunk under the North Saskatchewan River. Although the majority of the Riverview area can be serviced by gravity trunks, there are several areas along the bank of the North Saskatchewan River that will require pumping of sanitary flows. The sanitary servicing network concept is identified on Figure 11 – Sanitary Servicing.

An interim sanitary system will be required if the SESS river crossing is not constructed at the time initial development of Riverview occurs. The interim system will consist of a sanitary pump station located near the future terminus of the river trunk with a forcemain that crosses the Wedgewood Ravine and pumps wastewater northward to the existing Lessard Road trunk. Wet weather storage of wastewater may be required depending on the amount of land that is to be serviced in advance of the SESS connection.

### **Stormwater Drainage**

Stormwater management for the Riverview basin will consist of a series of interconnected management facilities with controlled discharge that will provide storage for peak events as well as treatment of stormwater prior to release into existing watercourses. The majority of the Riverview area will discharge to the North Saskatchewan River through a series of outfalls, with a small area in the northwest discharging to Wedgewood Creek. The general direction of stormwater drainage to each of these watercourses is shown on Figure 12 – Stormwater Servicing. The number of outfalls will be minimized through interconnection of the stormwater management facilities.

The stormwater management facilities will be designed as constructed wetlands to enhance water treatment prior to release. Low Impact Development principles related to stormwater management will be implemented wherever feasible to increase infiltration of stormwater, improve cleansing, and help manage runoff rates.

Several stormwater outfalls to the North Saskatchewan River from the Riverview area will be located upstream of the EL Smith water treatment plant, and water quality monitoring at these outfalls will be required. The monitoring program will be developed in consultation with EPCOR Water as neighbourhood plans that will require outfalls to the river are undertaken.

Stormwater management facilities will also be designed as easily accessible community focal points within Riverview, including shared use paths and landscaping to enhance the overall quality of the public open space.

### **Stormwater Management Facilities (SWMF)**

Stormwater facilities, though not technically classified as parks, are considered an amenity area and integral part of the open space system. Besides the function of these facilities as part of the storm servicing network, the SWMFs provide important wildlife habitat, areas for passive recreation, and incorporate shared use paths to facilitate active transportation.

### **Water Servicing**

Water servicing for this area will primarily be provided from an extension of the 600 mm transmission main on 199 Street north of Wedgewood Ravine. A network of mains within the arterial and collector roadway network of the Plan area will provide water service throughout the area. A second water feed into the Riverview area will be provided from a 400 mm connection to the Edgemont neighbourhood on Winterburn Road (215 Street).

The 199 Street arterial roadway crossing of Wedgewood Ravine is identified in the current ARA Bylaw as a bridge structure. The ravine crossing of the watermain will be reviewed with further study at the NSP stage. The Wedgewood Ravine crossing on Winterburn Road may also consist of a bridge structure. The ravine crossing at this location would also be reviewed in further study at the NSP stage.

Several areas within the Riverview ASP will require accommodation of higher than standard pressures. This may be accomplished through inline or in-home pressure reducing valves or by other means to be determined at the detailed engineering drawing stage. The water servicing concept is shown on Figure 13 – Water Servicing, and will be further refined with Neighbourhood level Hydraulic Network Analyses, which will be required in support of future NSPs.

#### Technical Summary

An Area Master Plan and Hydraulic Network Analysis have been prepared in support of the Riverview ASP.

Objective	ASP Policy	Implementation
3.4.13 (a) To ensure that Riverview is serviced to a full urban standard	(i) Sanitary and stormwater servicing shall be provided in accordance with the approved Area Master Plan.	(i) A Neighbourhood Design Report shall be completed in support of future NSP(s).
	(ii) Water servicing to the Riverview area shall be provided in accordance with an approved Hydraulic Network Analysis.	(ii) Approval of engineering drawings and servicing agreements will be required prior to installation of water servicing.
	(iii) Shallow utilities will be extended into the Plan area as required.	(iii) Installation of shallow utilities will be executed through servicing agreements.
	(iv) Accommodate the development of an electrical substation to provide power for Riverview.	(iv) The location of the electrical substation will be determined through consultation with EPCOR Power at the NSP stage.
3.4.13 (b) To explore innovative and alternative servicing option where possible	Support innovative and alternative service delivery, if feasible and practical.	Opportunities for innovative and alternative servicing options will be explored at the NSP stage. The City of Edmonton will consider supporting projects which meet all identified requirements.
3.4.13 (c) To ensure that the Riverview ASP area is serviced to a full urban standard, using contemporary and emerging sustainable infrastructure solutions, and that “soft” services are provided.	Ensure that development within the Riverview ASP is within acceptable response times as determined by emergency services.	City Administration and agencies should work with Council to ensure planning and funding is in place to provide for the timely delivery of municipal services.

### 3.5 IMPLEMENTATION

It is anticipated that the Riverview ASP will develop over a period of approximately 30 years. Within this time period, it is important to provide guidance that will ensure that development occurs in a way that maximizes efficiency while minimizing disturbance to surrounding communities.

### 3.5.1 GENERAL STAGING

The Riverview ASP will be developed in five neighbourhoods as shown on Figure 5 – Neighbourhood Areas. Development is recommended to commence in the northern and eastern portion of the Plan area and extend to the west and south. The numbering of neighbourhoods as shown in Figure 5 – Neighbourhood Areas may not reflect the actual sequence of NSP submissions or development. Plan proponents may seek authorization from Council to prepare an NSP at their discretion.

As further studies are completed to service the area, the order of neighbourhood development may change. Further details regarding the ultimate staging of each of the neighbourhoods will be provided at the neighbourhood structure plan stage through detailed review and modification of the preliminary conceptual plan design in consideration of contemporary market demand.

The extension of services and the upgrade of roadways in the existing Country Residential subdivisions of the Riverview area, should it be desired, would be expected to occur at the expense of the residents.

### 3.5.2 STAGING INFRASTRUCTURE AND SERVICING

Each development phase will include stormwater management facilities in addition to necessary off-site water and sewer mains appropriately located within and adjacent to the Riverview ASP area.

Extending existing utilities from the north will provide shallow utility servicing including electric, telephone, cable television and gas.

### 3.5.3 REZONING AND SUBDIVISION

Existing zoning within the Riverview ASP includes, but is not limited to, lands zoned Agricultural (AG) and existing residential subdivisions zoned Rural Residential (RR). Rezoning and subdivision of lands are to conform to the land uses designated in the Riverview ASP and subsequent NSPs will be undertaken when necessary.

### 3.5.4 ECOLOGICAL NETWORK

To support the preparation of the Riverview ASP, an Ecological Network Report was completed.

## 3.6 STATUTORY PLAN AND POLICY CONTEXT

In 2008, the City's Strategic Plan, The Way Ahead was approved by Council providing a vision for Edmonton in 2040 and establishing strategic goals to provide a clear focus for the future. The Riverview ASP advances the vision of The Way Ahead through the directional plans; The Way We Grow, The Way We Move, The Way We Live and The Way We Green as well as the four principles which guide the development and implementation of The Way Ahead.

This section outlines the various plans and policies which are applicable to the Riverview ASP including the City of Edmonton's Municipal Development Plan (The Way We Grow), People Plan (The Way We Live), Environmental Plan (The Way We Green), and the Transportation Master Plan (The Way We Move). Future NSPs in the Riverview area will comply with this ASP and those guiding its preparation. Applicants seeking amendments to the ASP or applying for rezoning, subdivisions or development permits are required to consult the actual documents for specific guidance on detailed requirements as they apply to particular properties.

### 3.6.1 CAPITAL REGION GROWTH PLAN: GROWING FORWARD

The primary purpose of the Capital Region Growth Plan is to manage sustainable growth in a manner that protects the region's environment and resources, minimizes the regional development footprint, strengthens communities, increases transportation choices and supports economic development. The Capital Region Growth Plan: Growing Forward was approved by the Government of Alberta on March 11, 2010.

Capital Region Land Use Plan	Riverview ASP
<b>I. Protect the Environment and Resources</b>	
<b>A. Preserve and Protect the Environment</b>	
<b>Policy (i)</b> – Any development which may cause detrimental effects such as erosion or pollution to lakes, rivers, water bodies and shorelines shall be prohibited unless appropriate mitigative measures are implemented.	The Riverview ASP protects the North Saskatchewan River and Ravine System from erosion and pollution.
<b>Policy (ii)</b> – Any development which fragments contiguous natural features, functions and habitat, such as water systems, moraines, forests, wetlands and wildlife habitat and corridors shall be discouraged.	The Riverview ASP designates a number of Natural Areas as either Environmental Reserve or Municipal Reserve. Natural areas to be retained will be further reviewed for retention at the NSP stage. Development that interrupts wildlife passage will be discouraged and mitigated, where necessary.
<b>Policy (v)</b> – Support innovative design, construction and operational technologies and strategies which reduce emissions.	The Riverview ASP will explore the application of renewable energy sources, green building technology, low impact development, and innovative servicing strategies with City Administration.
<b>Policy (vi)</b> – Manage land use distribution patterns to reduce reliance on automobiles.	The Riverview ASP promotes a complete, walkable community by encouraging neighbourhood commercial services integrated with residential development and by locating higher density residential uses in proximity to transit centres and bus routes.
<b>C. Protect natural Resources</b>	
<b>Policy (ii)</b> – Prevent development incompatible uses adjacent to natural resources.	The Riverview ASP provides policies to prevent incompatible land uses adjacent to natural resource extraction sites.
<b>II. Minimize Regional Footprint</b>	
<b>A. Identify, Protect and Prioritize Lands for Regional Infrastructure</b>	
<b>Policy (i)</b> – Ensure that lands identified for regional infrastructure such as energy transmission, highways, municipal infrastructure, transit and related facilities are protected from incompatible development.	The Riverview ASP requires compatible development in the vicinity of the Anthony Henday Drive transportation corridor and the AltaLink utility corridor or a buffer area between incompatible uses.
<b>Policy (v)</b> – Encourage and support sustainable development within the region.	The Riverview ASP promotes sustainable development by promoting compact development and low impact development practices.
<b>B. Concentrate New Growth Within Priority Growth Areas</b>	
<b>Policy (i)</b> – Most new growth shall occur within priority growth areas.	The Riverview area is located in the Capital Region Board's Priority Growth Area "C <sub>w</sub> ", establishing a minimum net residential density



Capital Region Land Use Plan	Riverview ASP
	target of 30 units per net residential hectare. The Riverview ASP land use concept exceeds this density target.
<b>Policy (v)</b> – Priority growth areas shall incorporate intensive forms of development that significantly exceed existing development patterns.	The Riverview ASP requires a variety of higher types with higher proposed densities than existing development patterns in Edmonton.
<b>Policy (ix)</b> – Development on or near municipal boundaries shall be compatible and consistent with the policies of the Plan and shall not impede the sustainable delivery of infrastructure.	The Riverview ASP provides an appropriate transition to adjacent municipalities' land uses.
<b>D. Support Expansion of Medium and Higher Density Residential Housing Forms</b>	
<b>Policy (i)</b> – New residential development shall provide a greater proportion of higher density residential units.	The Riverview ASP proposes multiple housing types with a hierarchy of densities. More than 30% of the residential units in Riverview are either medium or high density. Higher density residential uses are strategically concentrated in proximity to commercial uses.
<b>Policy (ii)</b> – Support innovative housing designs and / or built forms within new and existing residential neighbourhoods.	
<b>Policy (iii)</b> – Greenfield developments shall make provision for a mixture of uses including a diversity of housing forms, community services, local retail and employment opportunities.	
<b>Policy (iv)</b> – Transit accessibility must be included in the design of all new developments.	The land use concept includes a transit centre integrated in the town centre area. More detailed planning for transit accessibility will be addressed at the NSP level to ensure new developments are accessible to public transportation.
<b>III. Strengthen Communities</b>	
<b>A. Create Inclusive Communities</b>	
<b>Policy (i)</b> – Encourage and support the establishment of social infrastructure throughout the region.	The Riverview ASP promotes the development of civic, recreational, and service facilities within the area.
<b>Policy (ii)</b> – Support initiatives to improve the livability of communities.	The Riverview ASP promotes the protection and enhancement of natural areas throughout the Plan area and preserves the North Saskatchewan River Valley and Wedgewood Creek Ravine.
<b>Policy (iii)</b> – Integrate uses with adjacent developments to improve connectivity and accessibility to local parks, open space, commercial, and community services.	The Riverview ASP provides access to the recreational activities in the North Saskatchewan River Valley and Ravine System, and natural areas throughout the Plan area.
<b>Policy (iv)</b> – Encourage co-location and/or shared use of compatible public service infrastructure, such as education facilities, parks and civic uses.	The Riverview ASP promotes the integration of uses, such as education facilities with active recreation areas, and public transit with commercial uses.
<b>B. Support Healthy Communities</b>	
<b>Policy (i)</b> - Support the implementation of present and future initiatives to create and enhance parks, trails and natural areas for public use.	The Riverview ASP promotes the development of parks and natural areas throughout the Plan area and along the North Saskatchewan River Valley.
<b>Policy (ii)</b> – Improve accessibility to community services by providing sidewalks, bicycle trails to encourage walking and	The land use concept includes recreational trails along the Top-of-Bank and a public

Capital Region Land Use Plan	Riverview ASP
cycling and locate these services within proximity to transit, where possible.	transit facility near the town centre. Active and public transportation facilities will be further developed at the NSP level to encourage active transportation options and reduce the dependence on personal vehicles in Riverview.
<b>Policy (iv)</b> - Encourage and support innovative and green design solutions for neighbourhoods and buildings.	The Riverview ASP incorporates low impact development practices and green development principles.
<b>C. Support Public Transit</b>	
<b>Policy (i)</b> – Provide a mix of higher intensity land uses along transit corridors, at nodes, and employment centres.	The Riverview ASP encourages medium and high density residential development in close proximity to the transit centre, town centre and along bus routes.
<b>Policy (iii)</b> – New developments shall be designed for connectivity and accessibility to transit facilities.	
<b>D. Support Innovative and Affordable Housing Options</b>	
<b>Policy (i)</b> - Municipalities shall provide for a diversity of housing needs for the entire population of the region.	The Riverview ASP promotes the development of a variety of housing types.
<b>Policy (ii)</b> – All residential developments shall provide a greater variety of housing types.	The Riverview ASP encourages a range of housing types, including single- and semi-detached, street oriented row housing, and low-rise medium density housing.
<b>Policy (iii)</b> - Encourage intensification opportunities within existing residential neighbourhoods.	The Riverview ASP provides direction for the future redevelopment of the existing Country Residential in the area.
<b>IV. Increase Transportation Choice</b>	
<b>A. Integrate Transportation Systems with Land Use</b>	
<b>Policy (ii)</b> – Ensure the integration of public transportation infrastructure and land use development	The transit centre in the Riverview ASP is adjacent to the commercial area to maximize the benefit of the facility.
<b>Policy (iii)</b> – Design transportation infrastructure to support multiple modes of transport.	The land use concept includes recreational trails along the Top-of-Bank and a public transit facility near the town centre. Active and public options will be further developed at the NSP level to reduce the dependence on personal vehicles in Riverview.
<b>Policy (iv)</b> – Support development of inclusive communities to reduce the need for travel.	
<b>B. Support the Expansion of Transit Service in Various Forms</b>	
<b>Policy (i)</b> – Expand and extend the level, quality and range of public transportation options available to serve the Region.	The Riverview ASP encourages multiple modes of transportation. Detailed design of the transportation network at the NSP level will accommodate multiple modes of transportation, including personal vehicle, bicycle, and public transportation.
<b>Policy (iv)</b> – Support multi-modal transportation options by providing multi-use streets sufficient to accommodate bicyclists, motorists and pedestrians.	
<b>Capital Region Growth Plan Addendum</b>	
<b>Riverview ASP</b>	
<b>II. Land Use</b>	
<b>2.3 Land Use Policies</b> Density Targets – Priority Growth Area “C <sub>w</sub> ”: 30 – 40 dwelling units per net residential hectare	The Riverview ASP proposes a residential density that exceeds the minimum growth target mandated by the Capital Region Board.

### 3.6.2 MUNICIPAL DEVELOPMENT PLAN: THE WAY WE GROW

The Municipal Development Plan (MDP), “The Way We Grow,” approved in May 2010, is the City’s strategic growth and development plan. Through its MDP, the City of Edmonton will shape the city’s urban form and direct the development and implementation of more detailed plans. The plan is closely integrated with the Transportation Master Plan (TMP) to achieve more coordinated decision making. The plan also includes a regional component which addresses the coordination of future land use, growth patterns and transportation systems with Edmonton’s neighbouring municipalities.

<b>Municipal Development Plan: The Way We Grow</b>	<b>Riverview ASP</b>
<p><b>3.1.1.8</b> – Proponents for a new Neighbourhood Structure Plan will seek Council’s authority to prepare the plan. The information supplied by the applicant and administration will allow Council to provide direction and permission in accordance with Council’s Vision.</p>	<p>When requesting Council’s authorization to prepare a Neighbourhood Structure Plan, proponents and Administration shall provide the required information to inform Council’s decision.</p>
<p><b>3.2.1.1</b> – Ensure a combination of single family and multi-family housing development potential is available for the next 30 years.</p>	<p>The Riverview ASP encourages a combination of single and multiple-family residential units, which are expected to be developed over the next 30 years.</p>
<p><b>3.2.1.3</b> – Achieve a balance between residential, industrial, commercial, institutional, natural and recreational land uses in the city through land development policies and decisions.</p>	<p>The Riverview ASP includes a mix of residential, commercial, institutional, business employment, and recreational uses, and, where feasible, integrates these uses with one another to promote a complete, walkable community.</p>
<p><b>3.2.1.7</b> – Preparation of Area Structure Plans is authorized for the Horsehills, Southeast and Southwest Urban Growth Areas, and shall only be approved following Council acceptance of, and adherence with the:</p> <ul style="list-style-type: none"> <li>• Growth Coordination Strategy;</li> <li>• Integrated Infrastructure Management Plan; and</li> <li>• Citywide Food and Agriculture Strategy.</li> </ul>	<p>The Riverview ASP will comply with the policies of the Growth Coordination Strategy, Integrated Infrastructure Management Plan; and the City-Wide Food and Agriculture Strategy.</p>
<p><b>3.2.1.8</b> – Area Structure Plans for the Horse Hill, Southeast and Southwest Urban Growth Areas shall include the following:</p> <ul style="list-style-type: none"> <li>• An area vision created with area landowners and other city stakeholder groups;</li> <li>• A urban agricultural section in support of the City-Wide Food and Agricultural Strategy; and</li> </ul> <p>Innovative approaches to food systems, ecological networks, green infrastructure provision, demand reduction strategies and eco-design for future residential, commercial, industrial, business agriculture and natural uses.</p>	<p>The vision and principles for the Riverview ASP was developed with the Stakeholder Advisory Group which consisted of city administration, area landowners and other city stakeholder groups. The group worked together through a visioning charrette as well as on several other occasions to refine the plan. An agricultural section including urban agriculture is included in the plan and where possible innovative approaches to servicing, etc. will be incorporated.</p>
<p><b>3.2.1.11</b> – Neighbourhood Structure Plans within the Northeast, Southeast or Southwest Urban Growth Areas shall require Council authorization and comply with the Growth Coordination Strategy, the Integrated Infrastructure Management Plan, and the City-Wide Food and Agriculture</p>	<p>Council authorization will be requested at the time of Neighbourhood Structure Plan (NSP) preparation. All NSPs will also be in compliance with the Growth Coordination Strategy, the Integrated Infrastructure</p>

Municipal Development Plan: The Way We Grow	Riverview ASP
Strategy.	Management Plan, and the City-Wide Food and Agriculture Strategy.
3.2.2.3 – Ensure City departments and agencies collaborate to identify all municipal land needs within an Area Structure Plan, Neighbourhood Structure Plan or Area Redevelopment Plan boundary prior to plan approval.	All City departments and agencies have been consulted throughout the preparation of the Horse Hill ASP. All future NSPs will also require consultation prior to Council approval.
3.2.2.4 – Ensure all City departments identify facility development and service delivery needs prior to the approval of new plans and work collaboratively to prepare a comprehensive facility concept that will assist with budget planning.	Through consultation with City departments and agencies facility development and service delivery needs have been identified and incorporated into the Horse Hill ASP. The Growth Coordination Strategy and Integrated Infrastructure Management Plan will help in facility planning and budgeting.
3.6.1.6 – Support contiguous development and infrastructure in order to accommodate growth in an orderly and economical fashion.	The staging plan (Figure 14 – Staging Concept ) for Riverview outlines a logical and orderly progression of contiguous development from north to south to follow the efficient expansion of infrastructure.
4.3.1.1 – The City of Edmonton will take municipal reserve, school reserve or municipal and school reserve, or cash-in-lieu in accordance with the Municipal Government Act and will use the land or money for purposes as defined by the Municipal Government Act.	The land use concept outlines the area allocated for Municipal Reserve in the Plan area.
4.3.1.5 – Time the development of parks as closely as possible with the development they are intended to serve.	Parks and open spaces will be explored at the NSP level and developed to coincide with residential development in each neighbourhood.
4.4.1.1 – Provide a broad and varied housing choice, incorporating housing for various demographic and income groups in all neighbourhoods.	The Riverview ASP encourages a range of housing types to accommodate a variety of age groups and family types with a range of incomes.
4.5.1.1 – Work proactively and in partnership with others to meet a wide range of affordable housing needs in all areas of the city with a focus on LRT stations and transit centres.	The Riverview ASP encourages medium and high density housing options in close proximity to transit facilities and a number of these units will be allocated for affordable housing.
4.5.1.2 – Encourage new development and infill redevelopment to incorporate affordable housing that is visually indistinguishable from market housing.	The Riverview ASP includes a variety of housing types to provide affordable and high quality housing options.
4.6.1.3 – Ensure active transportation opportunities are included in plans and redevelopment proposals.	The ASP land use concept includes a Top-Of-Bank walkway along the North Saskatchewan River and Ravine System, and the ASP recommends active transportation connections between parks and open spaces. Additional active transportation options will be included in each NSP.
5.5.1.2 – Incorporate sustainable neighbourhood design principles, low impact development and ecological design approaches when planning and building new neighbourhoods.	The land use concept preserves and protects most natural areas in the Plan area and, where possible, prevents the fracturing of animal corridors with roadways.

Municipal Development Plan: The Way We Grow	Riverview ASP
	Detailed design at the NSP and subdivision level will incorporate Low Impact Design strategies and other sustainable development principles to ensure Riverview strengthens Edmonton's ecological network.
5.6.1.4 – Design density, land uses and buildings to benefit from local transit service by minimizing walking distances to transit service and by providing safe and comfortable pedestrian streetscapes and high quality transit amenities.	Medium and high density residential development is encouraged in the area surrounding public transit facilities and bus routes.
5.6.1.7 – Identify and preserve public views and vistas of the North Saskatchewan River Valley and Ravine System as new development occurs and require public access in accordance with the Top-of-bank Policy.	The ASP acknowledges the City's Top-of-Bank Policy, and has identified an approximate top-of bank line. Subsequent stages of planning will establish an urban development line and will specify the location of public roadways, shared use paths, viewpoint parks and other features according to approved policy to preserve views and public access.
5.7.1.1 – Design streets, sidewalks and boulevards to provide safe, accessible, attractive, interesting and comfortable spaces for pedestrians, cyclists, automobiles and transit and to accommodate utilities, landscaping and access requirements for emergency response services.	The Riverview ASP encourages high-quality urban design and landscaping of streets and public spaces to contribute to a vibrant and walkable public realm.
6.1.1.3 – Include retail space as a key component of planned mixed-use centres that focus on LRT stations and transit centres.	The Riverview ASP allows for mixed use development with retail as a key component in the area around the transit station.
6.2.1.11 – Accommodate home-based businesses and services.	The Riverview ASP allows for minor home-based businesses in the Plan area.
7.1.1.5 – Acquire critical natural linkages and buffer zones to ensure natural areas of ecological value remain sustainable within the urban context.	The Riverview ASP recognizes the importance of the Ecological Network and will ensure ecological linkages will be maintained and enhanced.
7.1.1.7 – Public projects, new neighbourhoods and developments will protect and integrate ecological networks, as identified in the Natural Connections Strategic Plan, by adopting an ecological network approach to land use planning and design.	The land use concept protects several Natural Areas and designates these areas as either Environmental Reserve or Municipal Reserve.
7.1.1.10 – Utilize the full legislative entitlements of environmental reserve, in accordance with the <i>Municipal Government Act</i> , during the land development process.	The Riverview ASP identifies potential environmental reserve which will be dedicated at the subdivision stage.
7.1.1.12 – Lands and features that meet the definition of environmental reserve, but are not claimed by the Province, should be taken by the City as environmental reserve and protected.	The Riverview ASP acknowledges that lands and features which meet the definition and intent of environmental reserve as per the MGA should be taken by the City.
7.3.3.2 – Maintain adequate separation between new urban developments and the North Saskatchewan River Valley and Ravine System through the City's Top-of-bank Policy, with	The Riverview ASP contains policies to protect the River Valley and Ravine System. A top-of-bank walkway will be identified at the NSP

Municipal Development Plan: The Way We Grow	Riverview ASP
viewscales and public access to the River Valley preserved.	stage in accordance with City policy.
7.4.1 – Utilize parks and open spaces to complement and enhance biodiversity, linkages, habitat and the overall health of Edmonton’s ecological network.	The Riverview ASP aims to link schools, parks and open spaces to create a well-functioning ecological network that enhances biodiversity, linkages, and protects habitats.
7.4.1.1 – Link parks and open spaces with natural systems through development and design to strengthen the connectivity of Edmonton’s ecological network, where feasible.	The land use concept protects Natural Areas in the Plan area by designating these areas as either Municipal Reserve or Environmental Reserve. Where feasible, these natural areas are connected with one another by way of green corridors, greenways, stormwater management facilities, parks, buffers, utility rights-of-way and other semi-natural landscape elements.
7.5.1.1 – Require new development to demonstrate that it has incorporated ecological design best-practices into the design of neighbourhoods and buildings to reduce stormwater run-off.	The Plan encourages the use of Low Impact Development principles in neighbourhood land use and servicing plans.
7.5.3.3 – Integrate indigenous vegetation, specifically low-maintenance drought tolerant species, and where feasible include edible plant species into City and private landscaping.	The Riverview ASP encourages the use of native, naturalized and edible plant species in both private and public spaces.
8.1.7.3 – Upon provincial approval of the Capital Region Plan Addendum, Edmonton’s new Area Structure and Neighbourhood Structure Plans in the Capital Region Plan’s priority growth area B, F, Cw or Ce will be required to meet or exceed the Capital Region’s minimum density targets.	The Riverview area is located in the Capital Region Board’s Priority Growth area “C <sub>w</sub> ,” establishing a minimum net residential density target of 30 units per net residential hectare. The Riverview ASP land use concept exceeds this minimum residential density target.
9.2.1.1 – Apply City of Edmonton Policy C515 Oil and Gas Facilities regarding setbacks, risk management and urban development.	City Policy C515 is acknowledged in the Riverview ASP. Neighbourhood Structure Plans and subdivision plans will confirm setback requirements.
9.3.1.4 – In consultation with the Energy and Resources Conservation Board (ERCB), ensure development setbacks from oil and gas pipelines are achieved through the subdivision approval process.	Urban development in the vicinity of oil and gas pipelines will be planned in accordance with relevant City of Edmonton policies and procedures.  Government agencies and industry operators were consulted in the development of this plan.

### 3.6.3 TRANSPORTATION MASTER PLAN: THE WAY WE MOVE

The Transportation Master Plan (TMP), The Way We Move, is the framework that responds to the City of Edmonton’s future transportation needs. The TMP directs policies and gives guidance for funding projects and programs that work towards an integrated transportation network. The TMP strives to ensure transit sustainability and increase transit

ridership, improve travel options to reduce barriers between different modes of transportation, increase traffic safety and manage traffic congestion to facilitate travel through and around the city.

TMP Strategic Goal	Riverview ASP Compliance with Strategic Goal
<p><b>Transportation and Land Use Integration</b> The transportation system and land use/urban design complement and support each other so that the use of transit and transportation infrastructure is optimized and supports best practices for land use.</p>	<p>The ASP land use concept features a town centre with a mix of commercial, medium and high density residential, and recreation uses in close proximity to public transportation facilities.</p>
<p><b>Access and Mobility</b> The transportation system is interconnected and integrated to allow people and goods to move efficiently throughout the city and to provide reasonable access with a variety of modes for people across demographic, geographic, socio-economic and mobility spectrums.</p>	<p>The transportation network encourages multiple modes of transportation, including personal vehicle, public transportation, and active transportation to connect residents with local commercial use and recreational amenities as well as those across Edmonton.</p>
<p><b>Transportation Mode Shift</b> Public transportation and active transportation are the preferred choice for more people making it possible for the transportation system to move more people more efficiently in fewer vehicles.</p>	<p>The Riverview ASP encourages medium and high density residential development in close proximity to the transit facility in the town centre or along major bus routes, encouraging fewer personal vehicle trips. Future NSPs will explore opportunities for Transportation Demand Management and other traffic management strategies.</p>
<p><b>Sustainability</b> Transportation decisions reflect an integrated approach to environmental, financial and social impacts thereby creating sustainable, liveable communities that minimize the need for new infrastructure and increase residents' quality of life.</p>	<p>The Riverview ASP supports a more compact and efficient use of land and Low Impact Development strategies, minimizing infrastructure investment beyond those necessary and encouraging active transportation.</p>
<p><b>Health and Safety</b> The transportation system supports healthy, active lifestyles, and addresses user safety and security including access for emergency response services, contributing to Edmonton's liveability.</p>	<p>The land use concept includes recreational trails along the Top-of-Bank of the North Saskatchewan River. Additional active transportation options will be included in each NSP.</p>

### 3.6.4 EDMONTON'S PEOPLE PLAN: THE WAY WE LIVE

Edmonton's People Plan, The Way We Live, will provide residents, community stakeholders and the City with a blue print for the coordination and delivery of people services in order to advance and support the 10-year goal of improving Edmonton's Livability. Edmonton is one of Canada's most livable cities because it is welcoming to all, safe and clean, fosters its heritage and supports its arts and multicultural communities, encourages active lifestyles through recreation opportunities and engages its citizens in the city vision and direction. The Riverview ASP recognizes the six goals of The Way We Live:



Goal	Riverview ASP
Edmonton is a Vibrant, Connected, Engaged and Welcoming Community	The Riverview ASP provides opportunities in neighbourhoods, communities and public spaces to connect people and build vibrant communities. The Riverview ASP also integrates public transit with employment centres such as the town centre, areas of higher density as well as near the recreation centre.
Edmonton Celebrates Life	The Riverview ASP celebrates and promotes healthy living by providing for a variety of active and passive park space, the preservation of natural areas and connections to the North Saskatchewan River Valley and Ravine System. The Riverview ASP also celebrates and promotes both the agricultural and historical heritage of the area.
Edmonton is a Caring, Inclusive, Affordable Community	The Riverview ASP provides the opportunity for a variety of housing options for people throughout their life cycle and offers complete neighbourhoods with commercial, business employment, recreational and educational facilities.
Edmonton is a Safe City	The design of neighbourhoods and buildings within the Riverview ASP are to include Crime Prevention Through Environmental Design (CPTED) Principles to ensure safety. The Riverview ASP supports the development of facilities for emergency response services as identified by Fire and Police Services.
Edmonton is an Attractive City	The Riverview ASP takes pride in and showcases its green spaces and natural areas by providing linkages and connections to the North Saskatchewan River Valley and Ravine System, preserving and enhancing natural areas, as well as introducing additional linkages and open space.
Edmonton is a Sustainable City	The Riverview ASP supports sustainable development by providing opportunities for green infrastructure, complete communities, innovation in servicing, and a variety of housing and transportation options.

### 3.6.5 EDMONTON'S ENVIRONMENTAL PLAN: THE WAY WE GREEN

The Riverview ASP recognizes the 12 goals of The Way We Green:

Goal	Riverview ASP
Healthy Ecosystems – Land	The Riverview ASP meets the goal of Healthy Ecosystems – Land by ensuring preservation of Natural Areas, Wetlands and the North Saskatchewan River Valley and Ravine System, and by providing linkages and open space for residents to enjoy these areas.
Healthy Ecosystems – Water	The Riverview ASP contributes to the Healthy Ecosystem – Water goal by ensuring that stormwater is stored and treated naturally prior to release into the river. The Riverview ASP also encourages the exploration of Low Impact Development techniques.
Healthy Ecosystems – Air	The Riverview ASP meets the goal of Healthy Ecosystems – Air by including walkable neighbourhoods and transit service to reduce the use of the personal automobile as well as preserving Natural Areas where possible, or encouraging the planting of additional trees.
Energy and Climate Change	The Riverview ASP encourages further studies to be completed to determine the feasibility of District Energy facilities, green building technology and renewable energy generation.

Food	The Riverview ASP provides opportunities for the retention of private agricultural lands and incorporation and education of urban agriculture and peri-urban agriculture.
Solid Waste	The Riverview ASP area will continue promoting and adhering to the City of Edmonton Waste Management policies and regulations in reducing the amount of waste generated in the City.
Foundation for Success	The Riverview ASP strives to create a sustainable area within the City.

## 4 FIGURES

**FIGURE 1 – LOCATION**

**FIGURE 2 – LAND OWNERSHIP**

**FIGURE 3 – SITE CONTOURS**

**FIGURE 4 – SITE FEATURES**

**FIGURE 5 – NEIGHBOURHOOD UNITS**

**FIGURE 6 – LAND USE CONCEPT**

**FIGURE 7 – ECOLOGICAL CONNECTIVITY**

**FIGURE 8 – PARKLAND, RECREATION FACILITIES AND SCHOOLS**

**FIGURE 9 – ROADWAY NETWORK**

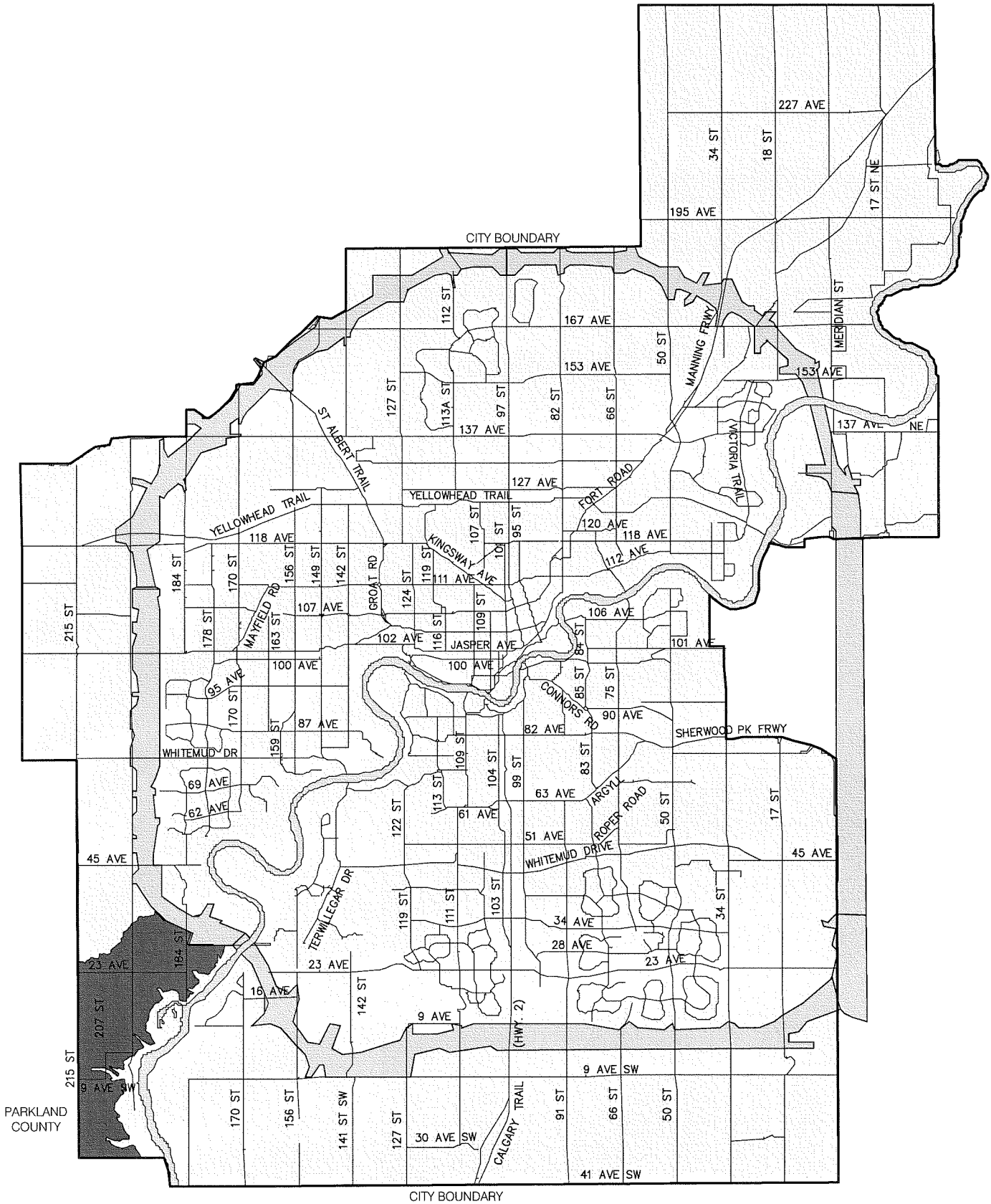
**FIGURE 10 – ACTIVE TRANSPORTATION NETWORK**

**FIGURE 11 – SANITARY SERVICING**

**FIGURE 12 – STORMWATER SERVICING**

**FIGURE 13 – WATER SERVICING**

**FIGURE 14 – STAGING CONCEPT**



LEDUC COUNTY

Legend


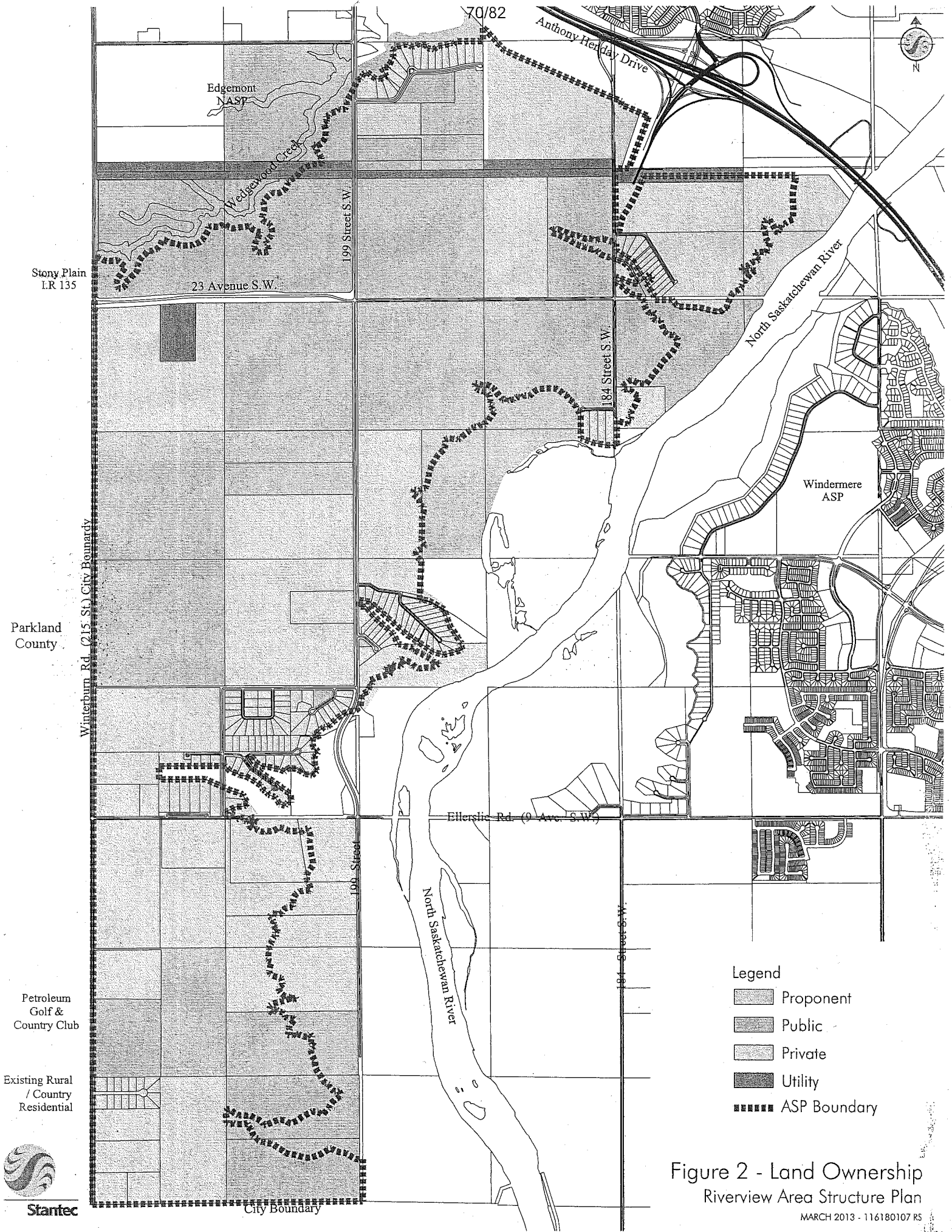
 Riverview ASP Boundary

Figure 1  
Location

Riverview Area Structure Plan

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Stony Plain  
I.R. 135

Edgemont  
N.A.S.P.

23 Avenue S.W.

199 Street S.W.

70/82

Anthony Herday Drive

North Saskatchewan River

Windermere  
ASP

Parkland  
County

Winterburn Rd. (215 St.) City Boundary

Ellerslie Rd. (9 Ave. S.W.)

100 Street

North Saskatchewan River

101 Street S.W.

Petroleum  
Golf &  
Country Club

Existing Rural  
/ Country  
Residential

City Boundary

Legend

- Proponent
- Public
- Private
- Utility
- ASP Boundary

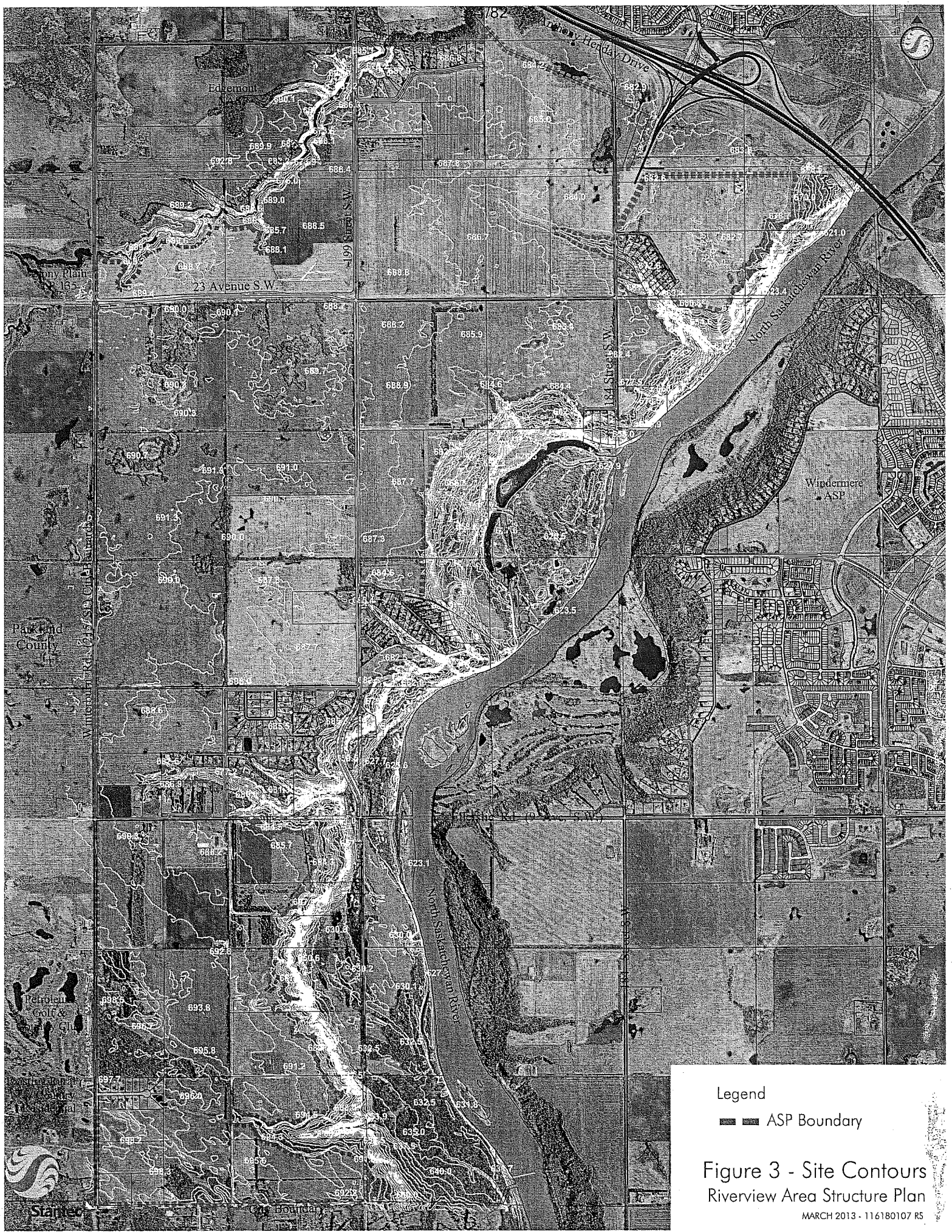
Figure 2 - Land Ownership  
Riverview Area Structure Plan

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Stantec





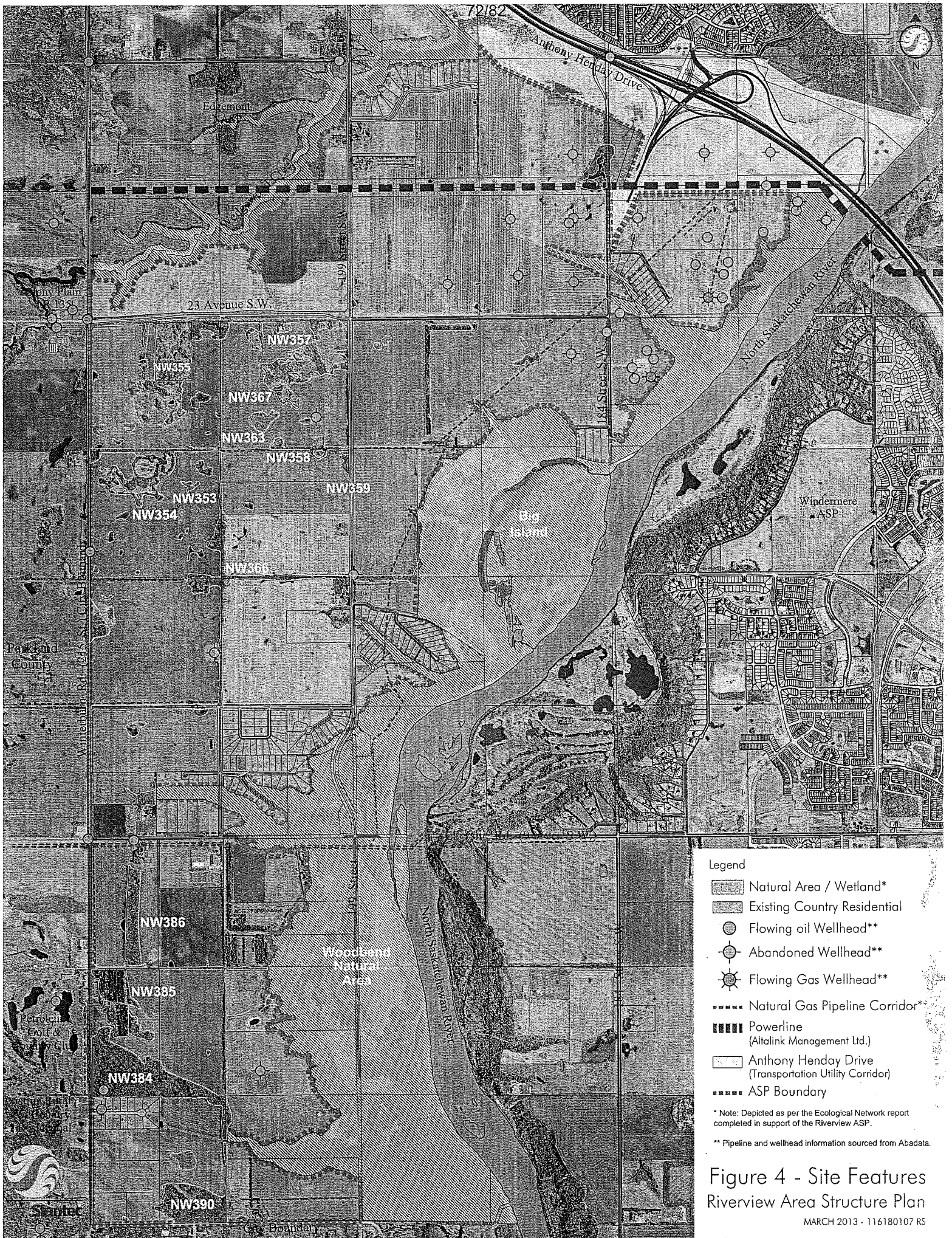
Legend

ASP Boundary










Figure 3 - Site Contours  
Riverview Area Structure Plan

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**Legend**

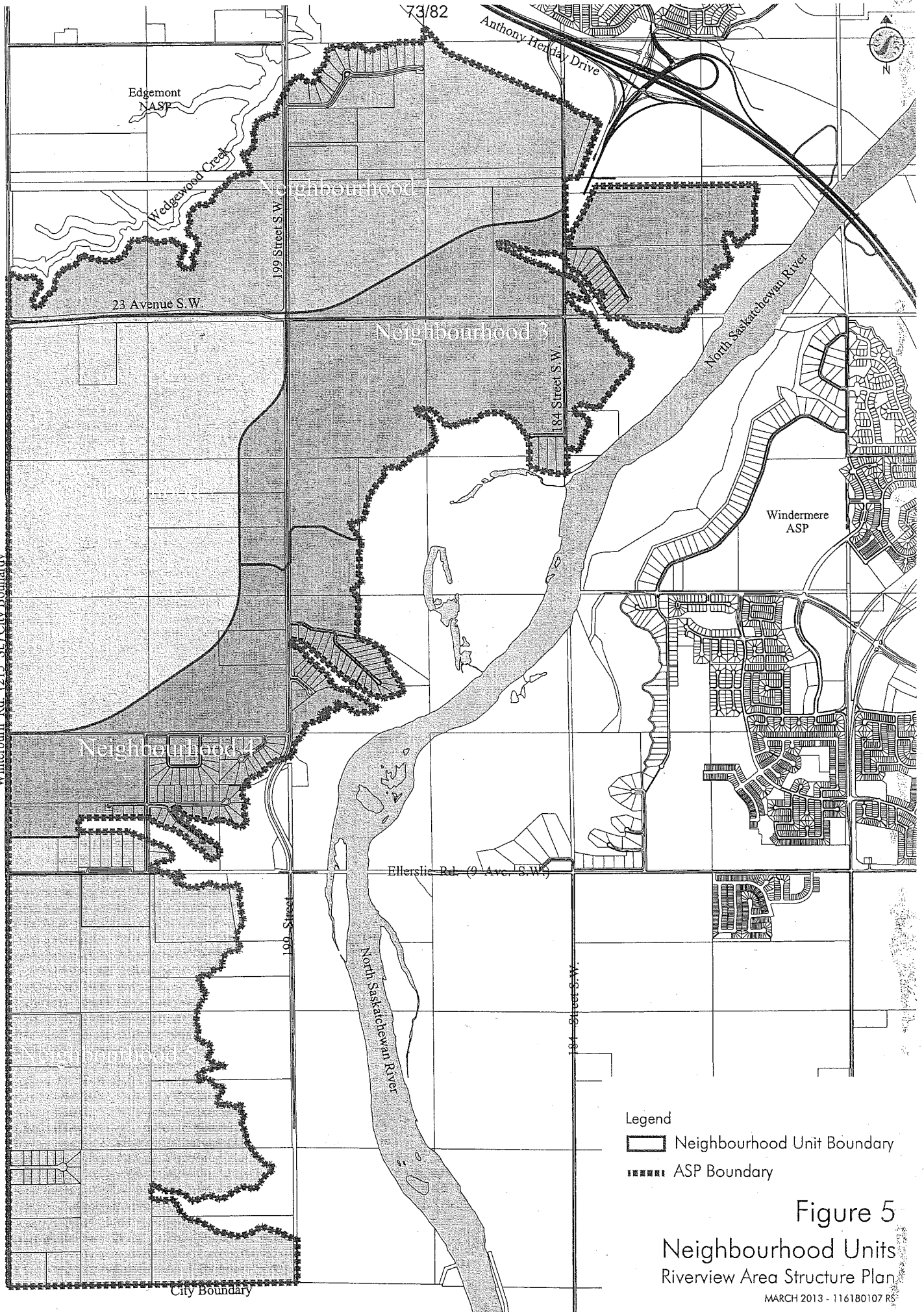
-  Natural Area / Wetland\*
-  Existing Country Residential
-  Flowing oil Wellhead\*\*
-  Abandoned Wellhead\*\*
-  Flowing Gas Wellhead\*\*
-  Natural Gas Pipeline Corridor\*
-  Powerline  
(Aitlink Management Ltd.)
-  Anthony Henday Drive  
(Transportation Utility Corridor)
-  ASP Boundary

\* Note: Depicted as per the Ecological Network report completed in support of the Riverview ASP.

\*\* Pipeline and wellhead information sourced from Abadeta.

**Figure 4 - Site Features  
Riverview Area Structure Plan**





Legend

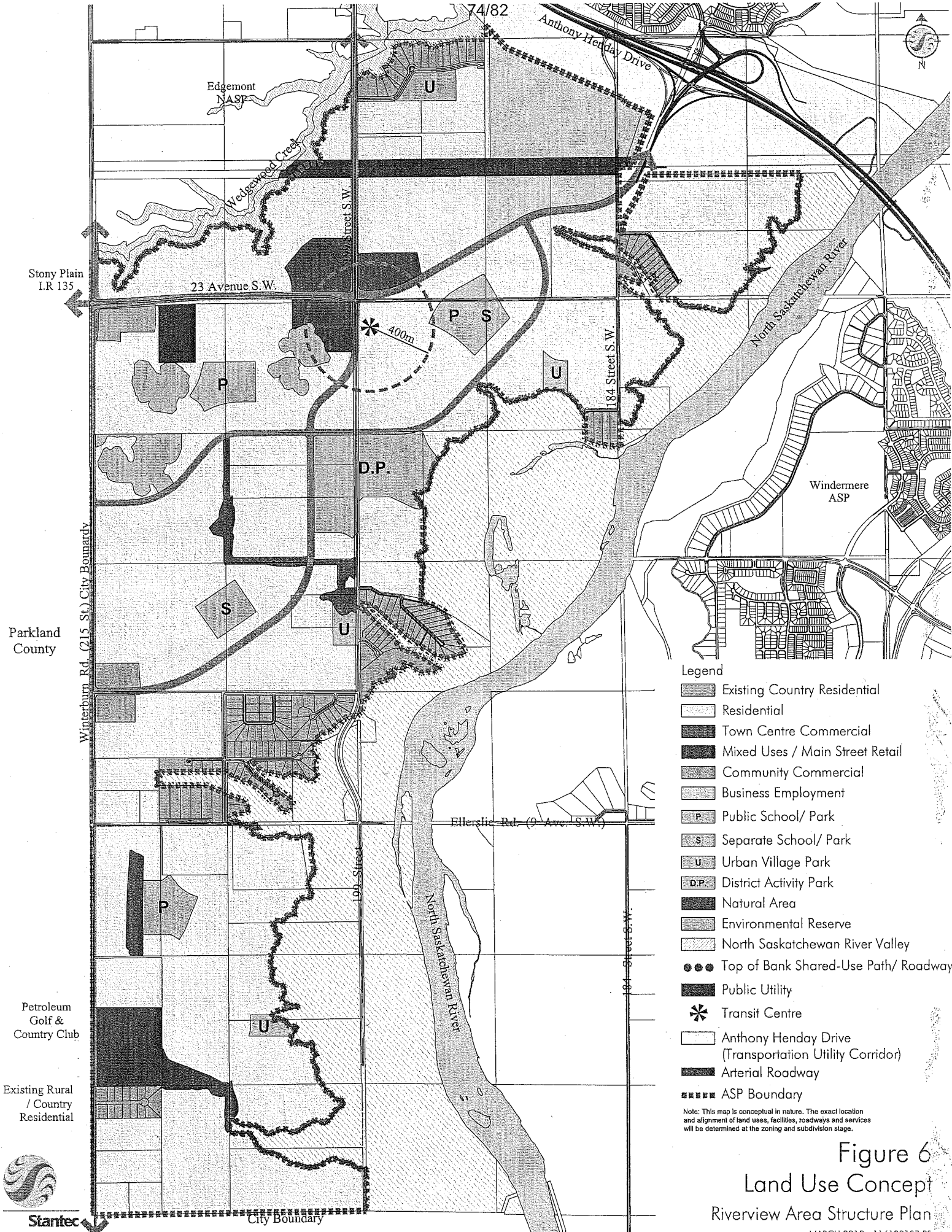
- Neighbourhood Unit Boundary
- ASP Boundary

Figure 5  
 Neighbourhood Units  
 Riverview Area Structure Plan

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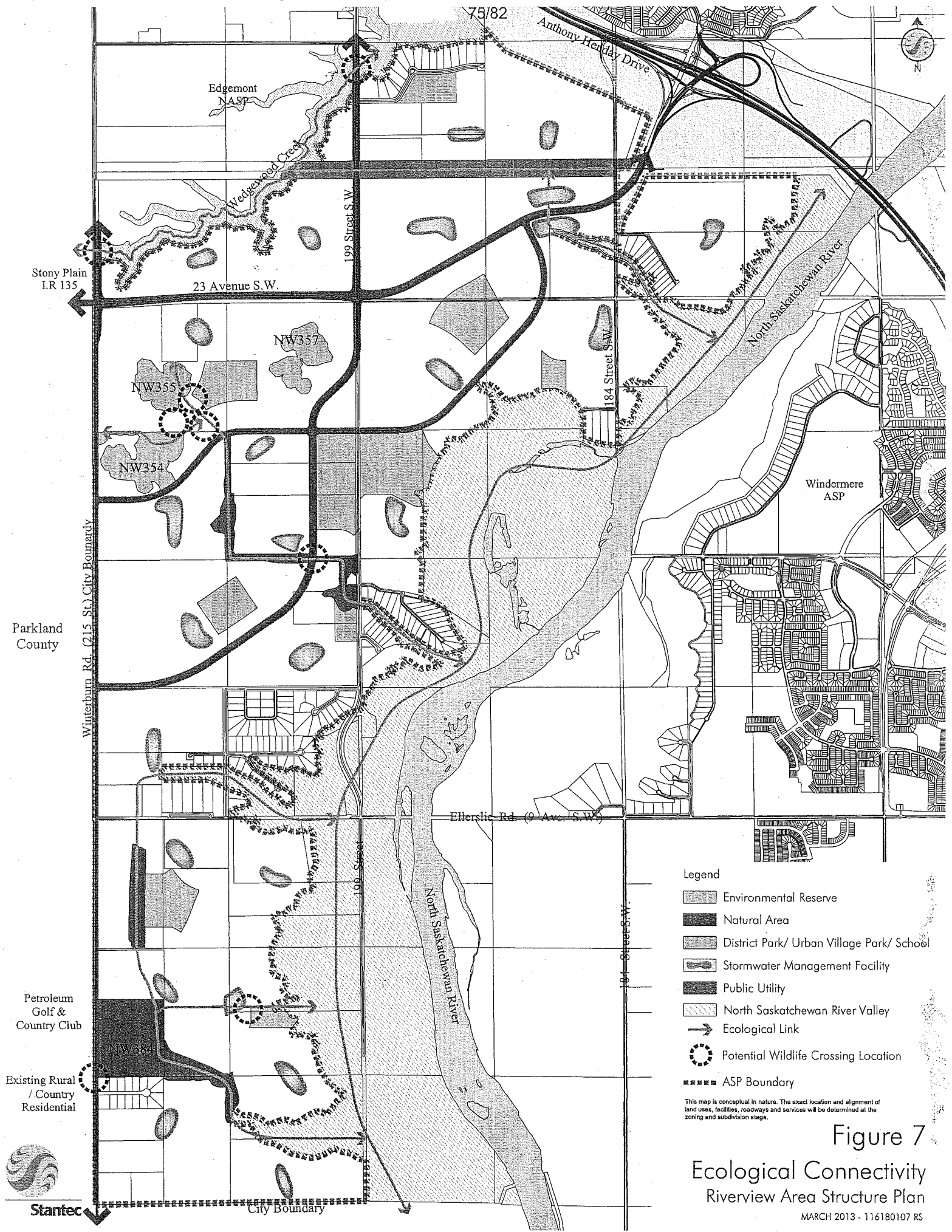


**Legend**

- Existing Country Residential
- Residential
- Town Centre Commercial
- Mixed Uses / Main Street Retail
- Community Commercial
- Business Employment
- P Public School/ Park
- S Separate School/ Park
- U Urban Village Park
- D.P. District Activity Park
- Natural Area
- Environmental Reserve
- North Saskatchewan River Valley
- Top of Bank Shared-Use Path/ Roadway
- Public Utility
- Transit Centre
- Anthony Henday Drive (Transportation Utility Corridor)
- Arterial Roadway
- ASP Boundary

Note: This map is conceptual in nature. The exact location and alignment of land uses, facilities, roadways and services will be determined at the zoning and subdivision stage.

**Figure 6**  
**Land Use Concept**  
 Riverview Area Structure Plan  
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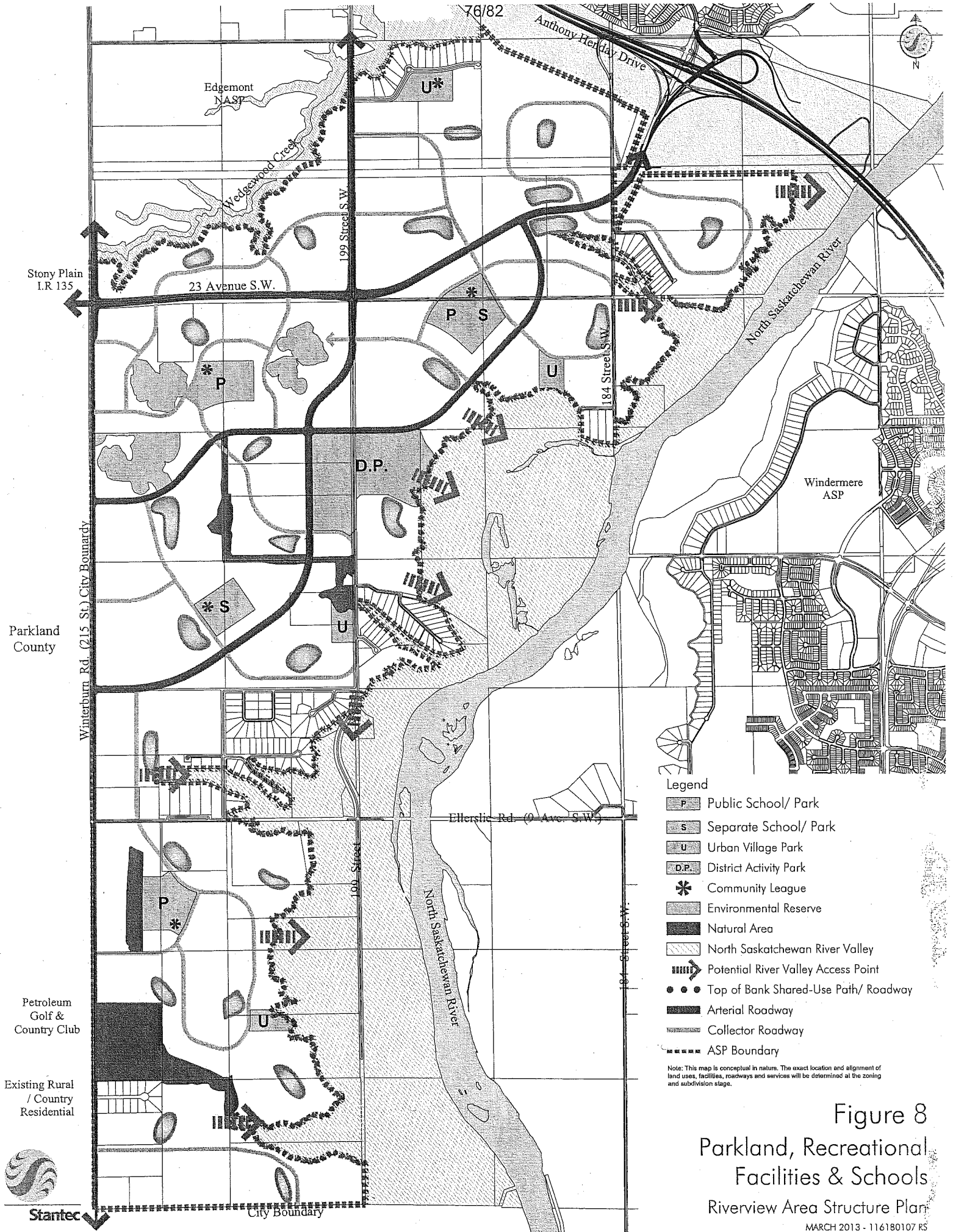
- Legend**
- Environmental Reserve
  - Natural Area
  - District Park/ Urban Village Park/ School
  - Stormwater Management Facility
  - Public Utility
  - North Saskatchewan River Valley
  - Ecological Link
  - Potential Wildlife Crossing Location
  - ASP Boundary

This map is conceptual in nature. The exact location and alignment of land uses, facilities, roadways and services will be determined at the zoning and subdivision stage.

**Figure 7**  
**Ecological Connectivity**  
 Riverview Area Structure Plan







- Legend**
- P Public School/ Park
  - S Separate School/ Park
  - U Urban Village Park
  - D.P. District Activity Park
  - \* Community League
  - Environmental Reserve
  - Natural Area
  - North Saskatchewan River Valley
  - Potential River Valley Access Point
  - Top of Bank Shared-Use Path/ Roadway
  - Arterial Roadway
  - Collector Roadway
  - ASP Boundary

Note: This map is conceptual in nature. The exact location and alignment of land uses, facilities, roadways and services will be determined at the zoning and subdivision stage.

**Figure 8**  
**Parkland, Recreational**  
**Facilities & Schools**  
**Riverview Area Structure Plan**  
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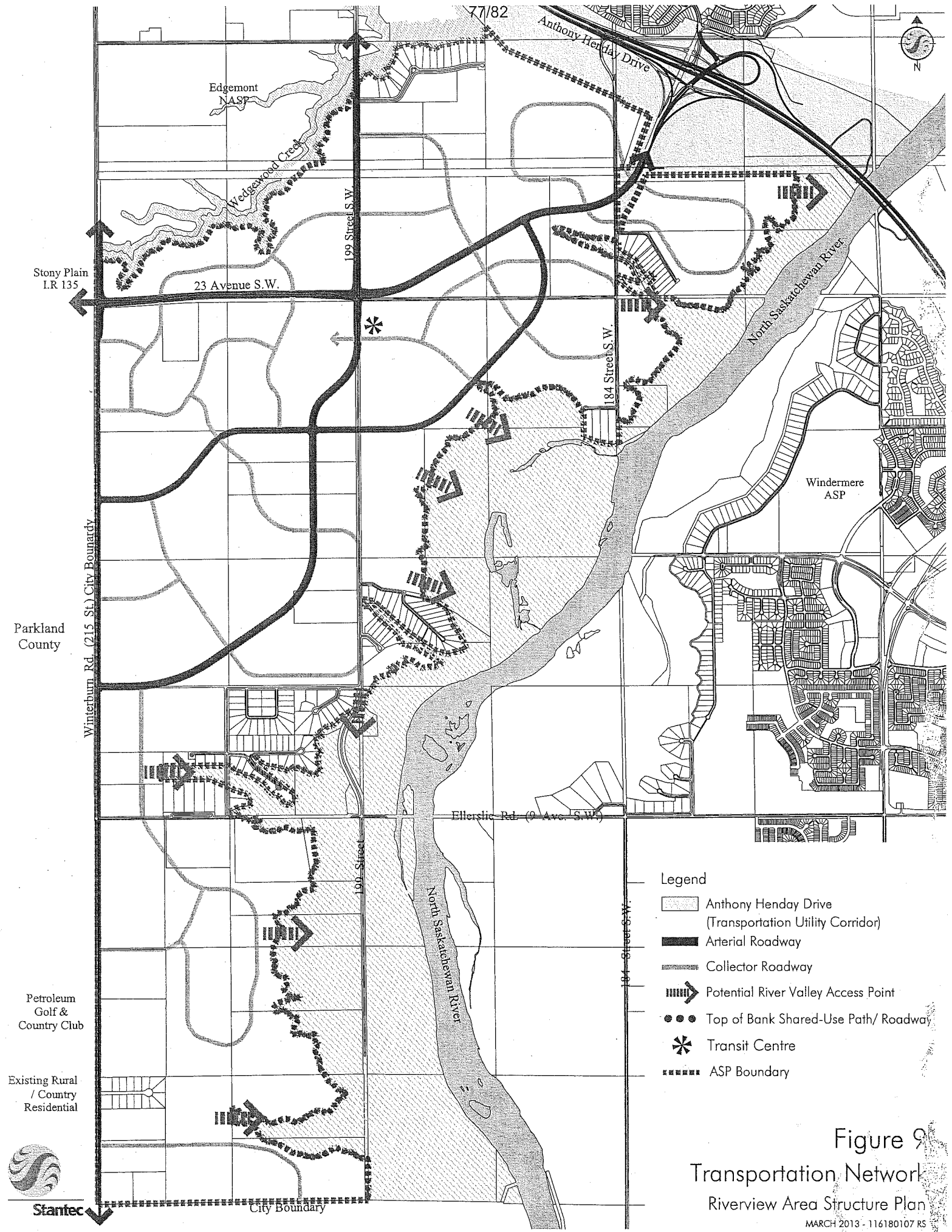


Figure 9  
 Transportation Network  
 Riverview Area Structure Plan  
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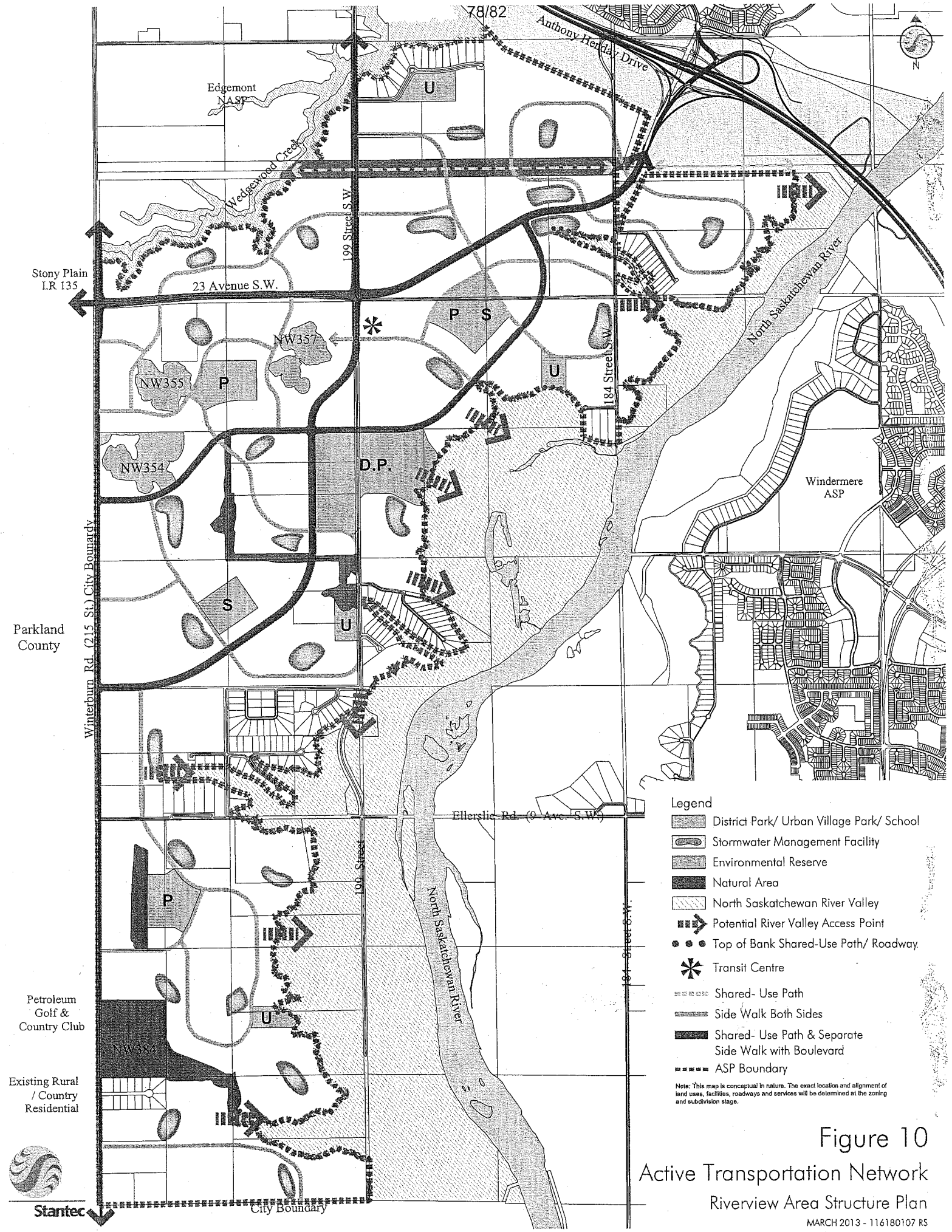
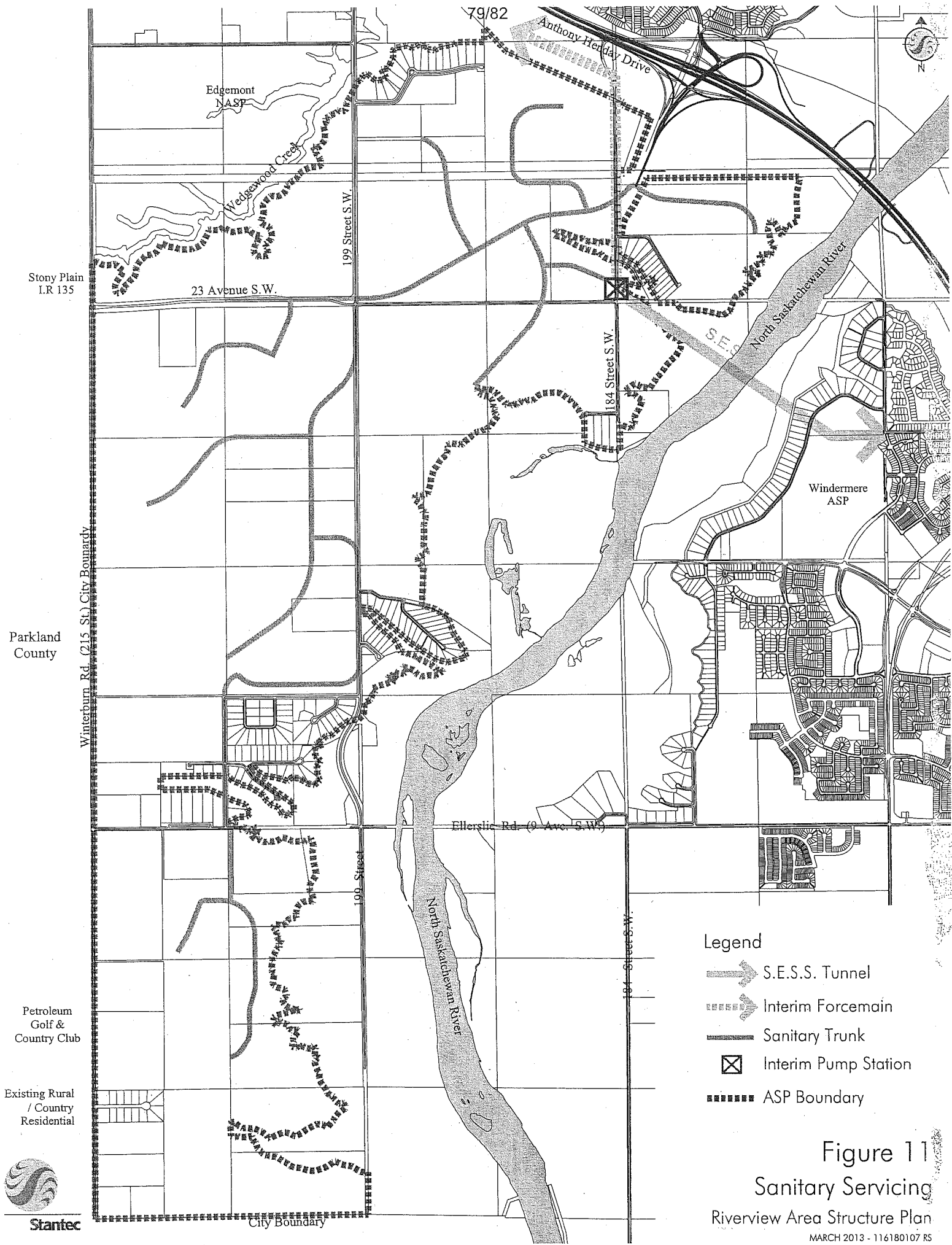


Figure 10  
 Active Transportation Network  
 Riverview Area Structure Plan  
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- Legend
-  S.E.S.S. Tunnel
  -  Interim Forcemain
  -  Sanitary Trunk
  -  Interim Pump Station
  -  ASP Boundary

Figure 11  
 Sanitary Servicing  
 Riverview Area Structure Plan  
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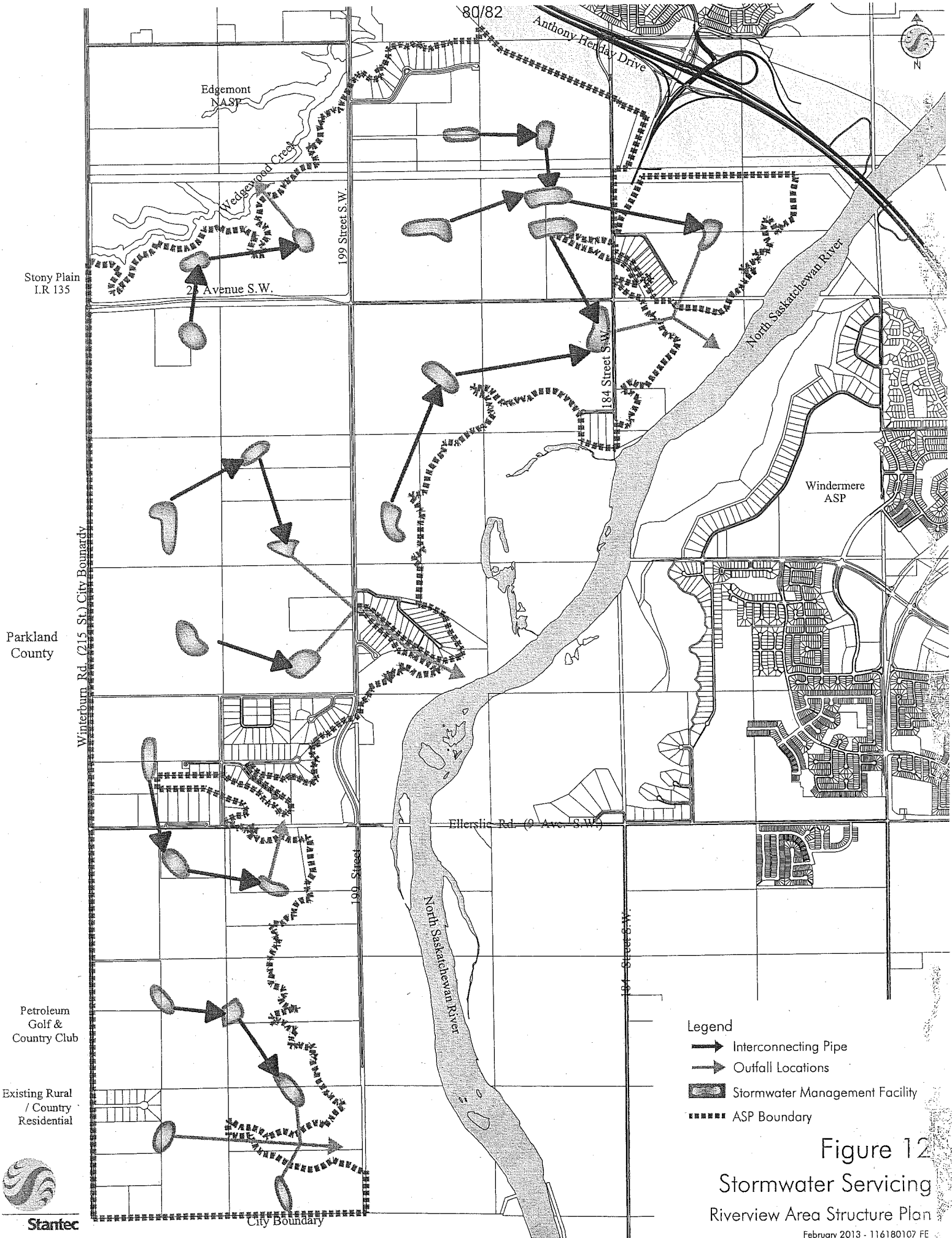
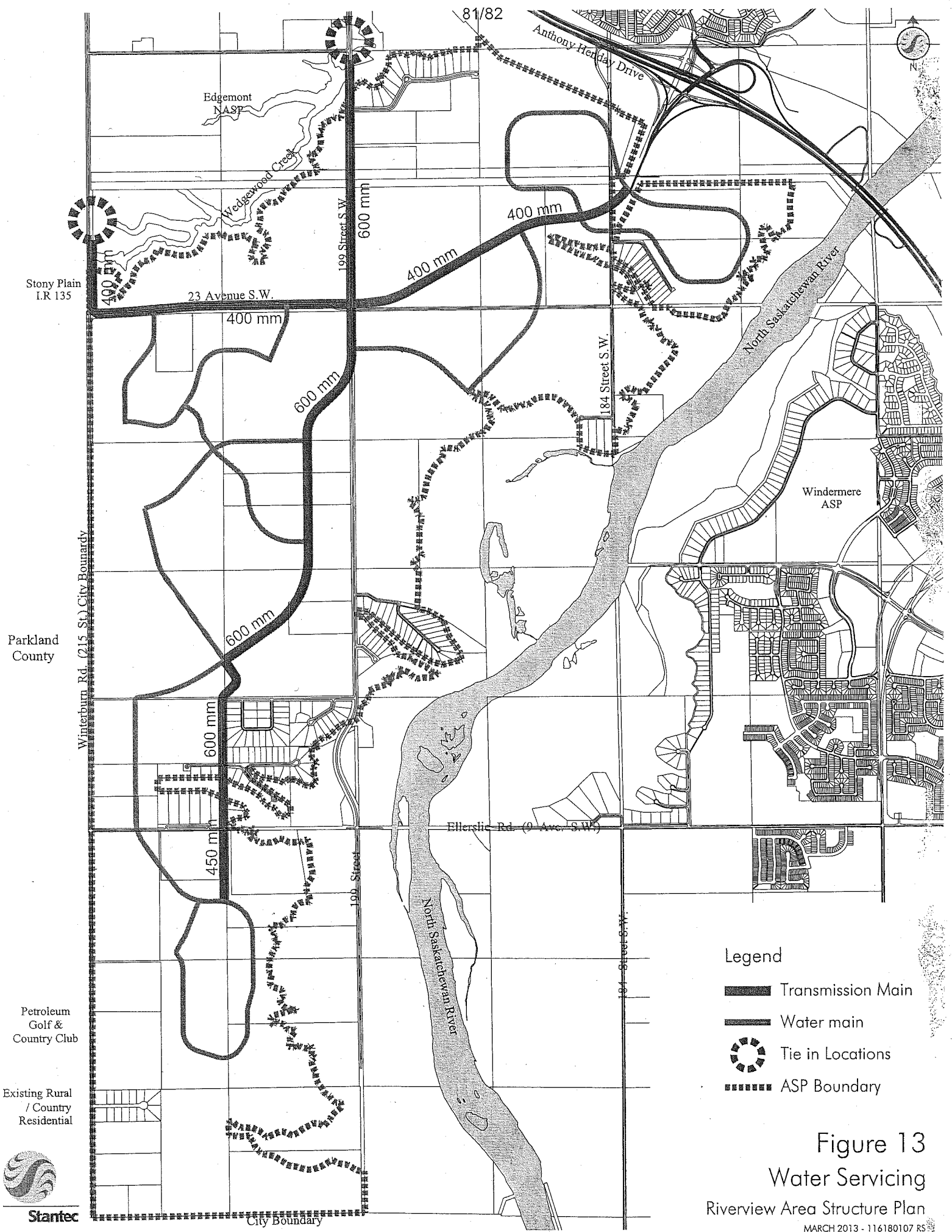


Figure 12  
 Stormwater Servicing  
 Riverview Area Structure Plan









- Legend
-  Transmission Main
  -  Water main
  -  Tie in Locations
  -  ASP Boundary

Figure 13  
 Water Servicing  
 Riverview Area Structure Plan  
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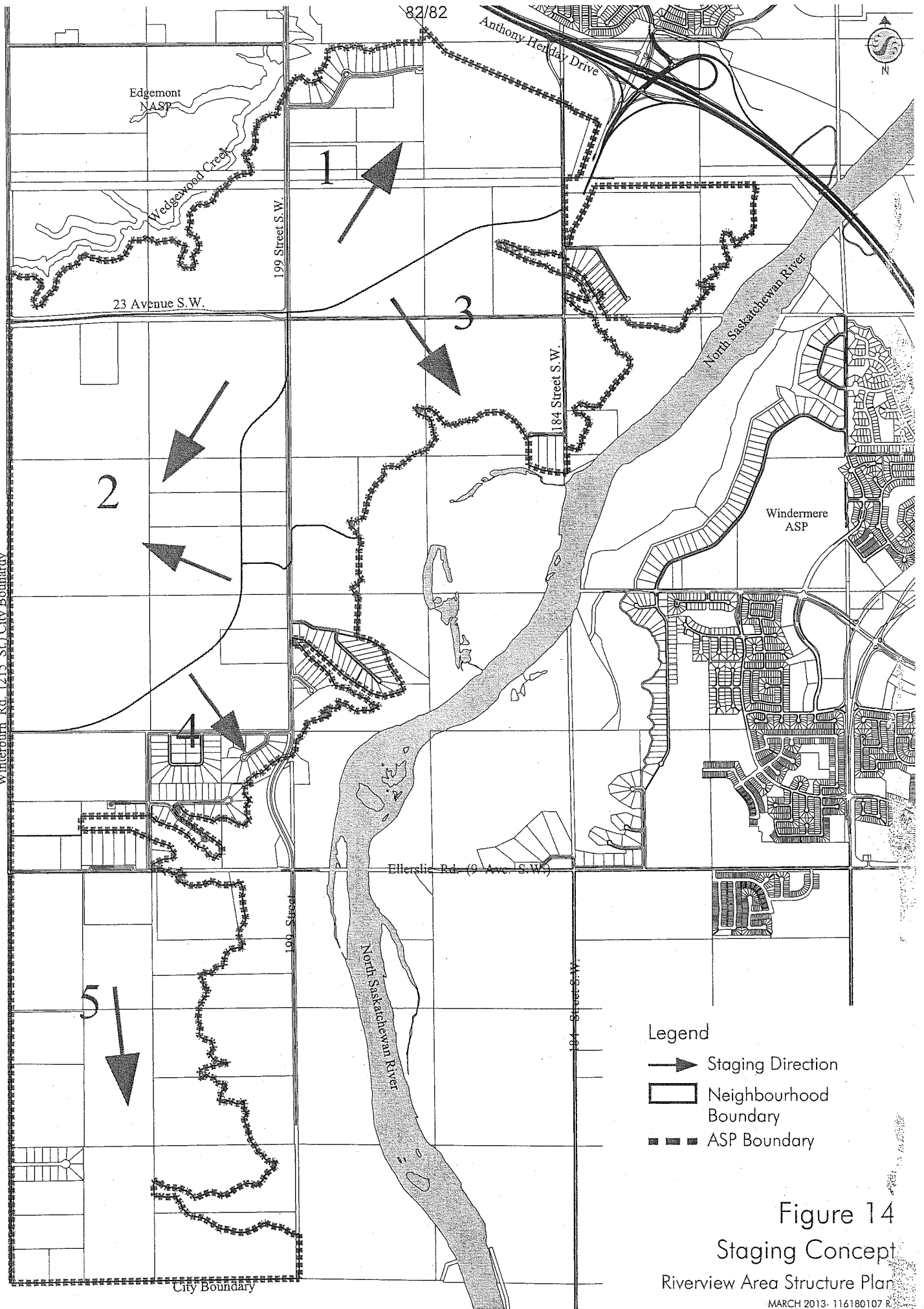


Figure 14  
 Staging Concept  
 Riverview Area Structure Plan  
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